

Indiana Department of Transportation

County Allen Route I-69 at SR 14 Des. No. 1401828

FHWA-Indiana Environmental Document

**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
General Project Information**

Road No./County:	Interstate (I-) 69 at State Road (SR) 14/Illinois Road / Allen County
Designation Number:	1401828
Project Description/Termini:	This interchange modification project will extend along SR 14/Illinois Road ('SR 14'), from Station (Sta.) 39+16.12 to Sta. 47+75.52 7+13.00, along the northwest outer ramp ('Ramp C') for 790 ft., and along the entire southwest loop ramp.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

<input type="checkbox"/>	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager)
<input type="checkbox"/>	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services Division)
<input checked="" type="checkbox"/>	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA
<input type="checkbox"/>	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA

Note: For documents prepared by or for Environmental Services Division, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval

_____	_____	_____	_____
ESM Signature	Date	ES Signature	Date
_____	_____	_____	_____
FHWA Signature	Date		

Release for Public Involvement

<u>N/A</u>	_____	<u>REB</u>	<u>12-11-18</u>
ESM Initials	Date	ES Initials	Date

Certification of Public Involvement

_____ Date _____
Office of Public Involvement

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

INDOT ES/District Env.

Reviewer Signature: _____ Date: _____

Name and Organization of CE/EA Preparer: Jessica Peterson, MS, Metric Environmental, LLC

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Part I – PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Does the project have a historic bridge processed under the Historic Bridges PA*?	Yes <input type="checkbox"/>	No <input checked="" type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.*

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e. notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

All survey activities were conducted within existing Indiana Department of Transportation (INDOT) right-of-way; therefore, notice of survey letters were not sent to adjacent property owners.

This project meets the minimum requirements in the INDOT *Public Involvement Manual*, which requires INDOT to offer the public opportunities to submit comments and/or request a public hearing. Therefore, a legal notice will appear in a local publication when this document is released for public involvement. This document will be revised after the public involvement requirements are fulfilled.

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Public Controversy on Environmental Grounds Will the project involve substantial controversy concerning community and/or natural resource impacts? Yes No X

Remarks: No known public controversy concerning community and/or natural resource impacts have been identified.

Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: INDOT INDOT District: Fort Wayne Local Name of the Facility: I-69 at SR 14

Funding Source (mark all that apply): Federal X State X Local Other*

*If other is selected, please identify the funding source:

PURPOSE AND NEED

Describe the transportation problem that the project will address. The solution to the traffic problem should NOT be discussed in this section. (Refer to the CE Manual, Section IV.B.2. Purpose and Need)

The purpose of this project is to improve safety of the subject interchange associated with the existing weaving conflicts between traffic on SR 14 and the southwest and southeast loop ramps. Weaving is defined as "the crossing of two or more traffic streams traveling in the same general direction along a significant length of highway without the aid of traffic control devices," (Indiana Design Manual, 2013).

The need for this project is due to weaving conflicts between traffic on SR 14 and the southwest and southeast loop ramps, as evidenced by two factors—(1) the crash frequency and (2) crash type distribution. The supporting evidence provided in the Engineer's Report by Strand Associates, Inc. (Strand) (2017) is summarized as follows (I-2 to I-4):

- (1) Crash frequency can be described in terms of Index of Crash Frequency (ICF), which is defined as the difference between expected crash frequency and the actual, reported crash frequency. The ICFs through the interchange ranged from values slightly below expected on the southwest loop ramp to significantly higher than expected at the northwest loop ramp (I-4). Strand concluded that the ICF values are indicative of a weaving problem between the loops and mainline traffic.
(2) Based on Strand's analysis of INDOT data, from 2012 through 2016, the crash type distribution showed three primary crash types— rear-end, ran-off-road, and same-direction-sideswipe. These three crash types can be attributed to high volumes of traffic merging and weaving. Per the American Association of State Highway and Transportation Officials (AASHTO) A Policy on Geometric Design of Highways and Streets (2001), when the sum of traffic volumes on any two adjacent loops in a cloverleaf interchange is 1,000 vehicles per hour (vph) or greater, traffic flow on the mainline is impaired, and weaving conflicts between the mainline and the loops are common. Strand's analysis of Northeastern Indiana Regional Coordinating Council (NIRCC) data indicates that the sum of the AM peak traffic volumes for the southeast and southwest loop ramps is approximately 1,800 vph.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE)

County: Allen Municipality: Fort Wayne

Limits of Proposed Work: The project will extend along SR 14, from Sta. 39+16.12 to Sta. 47+75.52 7+13.00, along Ramp C for 790 ft., and along the entire southwest loop ramp.

Total Work Length: 0.18 Mile(s) Total Work Area: 3.5 Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required? Yes1 No X Date: 11/13/2017

This is page 3 of 6 Project name: Interchange Modification Date: November 29, 2018

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¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

In the remarks box below, describe existing conditions, provide in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

This project is in the northwest portion of Fort Wayne in Aboite Township, Allen County, Indiana (B-1). Specifically, the project is in Sections 1 and 2, Township 30 North, Range 11 East within the Fort Wayne West, Indiana United States Geological Survey (USGS) 7.5' topographic quadrangle (B-2).

I-69, an Urban Interstate, consists of three 12-ft. wide travel lanes, a 12-ft. wide outside shoulder, and a 14-ft. wide inside shoulder in each direction, separated by a 2.5-ft. wide by 3.76-ft. high concrete barrier wall. The speed limit is 65 miles per hour (mph). SR 14, an Urban Principal Arterial, consists of two 12-ft. wide through lanes, a 12-ft. wide auxiliary lane provided at ramps, a 10-ft. wide outside shoulder, and a 6-ft. to 8-ft. wide asphalt median shoulder in each direction, separated by a 2.5-ft. wide by 2.75-ft. high concrete barrier wall. The speed limit is 45 mph. No sidewalks are present within the project limits. Both SR 14 and I-69 are on tangent sections. I-69 has a generally north-south alignment and SR 14 has a generally east-west alignment. SR 14 intersects I-69 at an 83-degree angle. The interchange is configured as a full cloverleaf with two I-69 exit ramps (one loop ramp and one outer ramp) and two I-69 entrance ramps (one loop ramp and one outer ramp) provided in each direction on SR 14. The northwest outer ramp is referred to as Ramp C in this document. All ramps except Ramp C have one 15-ft. to 17-ft. wide travel lane with variable width inside and outside shoulders. Ramp C has one 15-ft. to 17-ft. wide travel lane that divides into two 12-ft. wide lanes approximately 220 ft. north of the SR 14 stop line. The only traffic control present at the interchange is a signal at Ramp C to control its dual right-turn lanes and westbound SR 14 traffic. Land uses surrounding the area include residential and commercial (B-3 to B-12).

The preferred alternative is the 'Closed Loop' alternative, which consists of closing, deconstructing, and removing the southwest loop ramp; diverting traffic onto Ramp C; and replacing existing signage and signalization within construction limits (B-4 and B-13 to B-47). Ramp C will be widened to accommodate two new left-turn lanes, and the appropriate signage and signalization will be installed. Modern signalization will be installed to indicate two left-turn only lanes and two right-turn-only lanes at the Ramp C and SR 14 interchange. Modern signage will also be installed for through-traffic on I-69. The proposed typical section of Ramp C will include two 12-ft. wide left-turn lanes, two 12-ft. wide right-turn lanes, 4-ft. wide and 6-ft. wide paved inside and outside shoulders, respectively, and 3-ft. wide aggregate shoulders on both sides. The existing concrete median barrier on this section of SR 14 will be removed to allow for the new left-turn movements. An additional EB travel lane will be constructed on the south side of SR 14. The new SR 14 EB travel lane will be a shared right/through lane, beginning east of the southwest outside ramp at Sta. 40+19.13 and ending 95 ft. west of the SR 14 over I-69 bridge at Sta. 47+75.52. The new SR 14 EB cross section will provide three 12-ft. wide travel lanes, a 6 ft. wide median shoulder, and a 10 ft. wide outside shoulder. The new SR 14 travel lane and the new northwest outer ramp travel lane will both be constructed of hot mix asphalt full depth overlays within existing roadside grass. Project termini for the preferred alternative extend along SR 14 from Sta. 39+16.12 to Sta. 47+75.52 7+13.00, along Ramp C for 790 ft., and for approximately 1,200 ft. along the entire southwest I-69 loop ramp.

Five drainage structures will be installed or modified, as follows:

- Structure (Str.) Number (No.) 10 is an Inlet D-6, which will be removed, in the western portion of the construction zone at Sta. 39+80.47.
- Str. No. 11 is a 12-in. inlet H-5, which will be removed, in the east-central portion of the construction zone at Sta. 41+99.69.
- Str. No. 12 will require a pipe extension with 20 ft. of 12 in. pipe and a manhole in the east-central portion of the construction zone at Sta. 41+99.04.
- Str. No. 13 will require a new yard inlet and 17 ft. of 18 in. pipe in the northeast quadrant at Sta. 13+83.00.
- Str. No. 14 is an 18 in. Manhole Type C with catch basin casting, which will be capped in the northeast quadrant at Sta. 13+61.75.

Traffic on I-69 and SR 14 will be permitted during construction utilizing phased construction and temporary lane closures, as described in the Maintenance of Traffic (MOT) During Construction section of this document.

The preferred alternative is expected to satisfy the purpose and need of the project by eliminating existing safety hazards, including the primary weaving conflict between SR 14 EB traffic and the southeast and southwest loop ramps and the secondary weaving conflicts between traffic on SR 14 and the southwest and southeast loop ramps.

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Functional Classification: SR 14: Principal Arterial
 Current ADT: 30,749 VPD (2015) Design Year ADT: 39,205 VPD (2040)
 Design Hour Volume (DHV): 3,470 Truck Percentage (%) 4
 Designed Speed (mph): 45 Legal Speed (mph): 45

	Existing	Proposed
Number of Lanes:	4-6	5-7
Type of Lanes:	Two 12-ft. asphalt through lanes and one 12-ft. wide auxiliary lane (at ramps only) in each direction	EB: Two 12-ft. asphalt through lanes and one 12-ft. wide auxiliary lane (throughout the project area); Westbound: Two 12-ft. asphalt through lanes and one 12-ft. wide auxiliary lane (at ramps only)
Pavement Width:	40	52.5
Shoulder Width:	6.5 and 10	6.5 and 10
Median Width:	12-16	16
Sidewalk Width:	0	0

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

DESIGN CRITERIA FOR BRIDGES

Structure/NBI Number(s): N/A Sufficiency Rating: N/A
 (Rating, Source of Information)

	Existing	Proposed
Bridge Type:	N/A	N/A
Number of Spans:	N/A	N/A
Weight Restrictions:	N/A	N/A
Height Restrictions:	N/A	N/A
Curb to Curb Width:	N/A	N/A
Outside to Outside Width:	N/A	N/A
Shoulder Width:	N/A	N/A
Length of Channel Work:	N/A	N/A

Describe bridges and structures; provide specific location information for small structures.

Remarks: Construction limits terminate approximately 95 ft. west and 70 ft. south of the bridge that conveys SR 14 over I-69, INDOT Bridge No. 14-02-04545B, National Bridge Inventory No. 003860. No bridges are within construction limits; therefore, this project does not include any bridge work. One small structure, CV 014-022-122.80, a 320-ft. long by 15.3-ft. span by 9.9-ft. rise corrugated metal pipe arch, which conveys Durnell Ditch under SR 14, is located within the project area. However, no modifications to the existing culvert will occur as a result of this project. Five drainage structures will be installed or modified, as described in the Project Description (Preferred Alternative) section of this document.

Will the structure be rehabilitated or replaced as part of the project? Yes No N/A
If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

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MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: Both I-69 and SR 14 will remain open to traffic during construction. Phased construction with temporary lane and shoulder closures will be utilized. During phase 1, traffic on SR 14 EB and WB will be shifted to the outer lanes while the existing concrete barrier is removed. Traffic on Ramp C will remain at existing conditions and traffic devices will alert drivers of workers in the area. During phase 2, traffic on Ramp C will be shifted to the west side while full depth HMA pavement is constructed on east side of the ramp. During phase 3, traffic on SR 14 EB will be shifted to north while full depth HMA pavement is constructed on the south side.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences will cease upon project completion. The project sponsor will be responsible for contacting school districts and emergency services at least 2 weeks prior to construction. Delays would/may occur during construction but would cease with project completion.

ESTIMATED PROJECT COST AND SCHEDULE

Engineering: \$ 165,000 (2017) Right-of-Way: \$ N/A Construction: \$ 922,500 (2020)
 15,000 (2018)

Anticipated Start Date of Construction: March 1, 2020

Date project incorporated into STIP 2016-2019 Statewide Transportation Improvement Program (STIP), Amendment 16-01 (H-1), August 7, 2015. 2018-2021 STIP, Initial Version, July 3, 2017 (H-2).

Is the project in an MPO Area? **Yes** **No**

If yes,
 Name of MPO NIRCC

Location of Project in TIP 2016-2019 Transportation Improvement Program (TIP), Page 86 (H-3). 2018-2021 TIP, Page 74 (H-4).

Date of incorporation by reference into the STIP July 3, 2017

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RIGHT OF WAY		
Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.00	0.00
Commercial	0.00	0.00
Agricultural	0.00	0.00
Forest	0.00	0.00
Wetlands	0.00	0.00
TOTAL	0.00	0.00

Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition or reacquisition, either known or suspected, and there impacts on the environmental analysis should be discussed.

Remarks: The existing right-of-way ranges from approximately 30 to 80 ft. from the outermost edges of pavement within the project area (B-3 and B-28 to B-38). All construction will occur within existing right-of-way. No permanent or temporary right-of-way will be required to complete this project.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>	<u>Impacts</u>	
		<u>Yes</u>	<u>No</u>
Streams, Rivers, and Watercourses	X		X
Federal Wild and Scenic Rivers			
State Natural, Scenic or Recreational Rivers			
Nationwide Rivers Inventory (NRI) listed			
Outstanding Rivers List for Indiana			
Navigable Waterways			

Remarks:

In the Red Flag Investigation (RFI), signed by INDOT Environmental Services (ES) on January 5, 2018, Metric Environmental, LLC (Metric) identified two river and stream segments within the 0.5-mile search radius (E-2 and E-7). The nearest stream, Durnell Ditch, flows through the project area. Because the stream is entirely encapsulated, no impact is expected. No other streams, rivers, or watercourses were identified within or adjacent to the project area.

On December 9, 2017, Metric reviewed the Federal Wild and Scenic Rivers listings; State Natural, Scenic, or Recreational Rivers; Nationwide Rivers Inventory; and Indiana Outstanding Rivers and Navigable Waterways. No listed waterways were within or adjacent to the project area.

On August 30, 2017, Metric conducted a waters determination and delineation. The *Waters Determination Report* (Waters Report) was completed and approved by INDOT Office of Environmental Services (ES) on January 30, 2018 (F-1 to F-59). One stream, Durnell Ditch, was identified within the project study limits (F-22 to F-23). Durnell Ditch, a perennial stream, is encapsulated within the entirety construction limits. Durnell Ditch has an upstream drainage area of 2.302 square miles. Durnell Ditch exhibits significant nexus with Wabash River, a Section 10 Traditional Navigable Water (TNW); therefore, it is likely a water of the US. No excavation will occur within Durnell Ditch and no permanent or temporary fill will be discharged into the stream.

This project meets the criteria in the United States Fish and Wildlife Service (USFWS) *Interim Policy for the Review of Highway Transportation Projects in Indiana* (USFWS Interim Policy) (2013) for programmatic coordination (C-5 to C-7). Since no work will occur within streams, USFWS standard recommendations to

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minimize impacts to streams do not apply to this project.

On August 31, 2017, Metric generated an automatic Indiana Department of Environmental Management (IDEM) Proposed Roadway Construction Projects letter (C-8 to C-11). Since no work will occur within streams, IDEM's standard recommendations to minimize impacts to streams do not apply to this project.

In a letter, dated September 28, 2017, Indiana Department of Natural Resources, Division of Fish and Wildlife (IDNR, DFW) recommended not excavating in the waterway and implementing erosion control devices (C-16 to C-18). No streams will be impacted; therefore, the waterway recommendation is not applicable. The erosion control recommendation is in the Environmental Commitments section of this document.

Other Surface Waters	Presence	Impacts	
		Yes	No
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: In the RFI, dated January 5, 2018, Metric identified four lakes within the 0.5-mile search radius (E-3 and E-7). The nearest feature, a perennial stream is located approximately 0.03-mile southeast of the project area. Based on its distance from the project area, no impact is expected.

On August 30, 2017, Metric conducted a waters determination and delineation. The Waters Report was completed and approved by INDOT ES on January 30, 2018 (F-1 to F-59). Five roadside ditches (RSDs) were identified within the project study limits. No OHWMs were observed on the RSDs, so they are non-jurisdictional (F-14 to F-15). Construction limits will extend into the RSDs, which will result in approximately 200 ft. of impacts. Although permits will be required for other natural resource impacts, as discussed in the Permits Checklist section of this document, additional permits will not be necessary for the impacts to RSDs, since they are non-jurisdictional.

On August 31, 2017, Metric sent an Early Notification letter to the Allen County Municipal Separate Storm Sewer System (MS4) coordinator (C-4). No response was received.

No additional surface waters were mentioned within any of the agency responses to early coordination (Appendix C).

Wetlands	Presence	Impacts	
		Yes	No
	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0.515 acre(s) Total wetland area impacted: 0.005 acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments
Wetland A	PEM1A	0.010	0.002	Isolated wetland
Wetland B	PEM1A	0.178	0.000	Isolated wetland
Wetland C	PEM1A	0.056	0.000	Isolated wetland
Wetland D	PEM1A	0.022	0.000	Isolated wetland
Wetland E	PEM1A	0.033	0.000	Likely water of the US
Wetland F	PEM1A	0.142	0.000	Likely water of the US
Wetland G	PEM1A	0.074	0.003	Isolated wetland

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Documentation

ES Approval Dates

Wetlands (Mark all that apply)

Wetland Determination
 Wetland Delineation
 USACE Isolated Waters Determination
 Mitigation Plan

X
X

January 30, 2018
January 30, 2018

Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, business or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project not meeting the identified needs.

X

Measures to avoid, minimize, and mitigate wetland impacts need to be discussed in the remarks box.

Remarks:

In the RFI, signed by INDOT ES on January 5, 2018, Metric identified 19 wetlands within the 0.5-mile search radius and the nearest feature, a palustrine wetland, was 0.05-mile southwest of the project area (E-2 and E-7). Due to their distances from the project area, no impact to any of the mapped wetlands is expected.

On August 30, 2017, Metric conducted a wetland determination and delineation. The Waters Report, dated July 25, 2017, was approved by INDOT ES on January 30, 2018 (Appendix F). The project area was investigated for evidence of hydrophytic vegetation, hydric soil, and wetland hydrology. Seven wetlands, comprised of a total of 0.515 acre, were identified.

Wetland A is north of SR 14, between the I-69 southbound on and off ramps (F-7 and F-22). Wetland A is a 0.010-acre Palustrine, Emergent, Persistent, Temporarily Flooded (PEM1A) wetland. Wetland A does not exhibit significant nexus with any jurisdictional waters of the US, so should be classified as a Class I isolated wetland. Wetland A does not support significant wildlife or aquatic habitat or possess significant hydrologic function; therefore, it should be considered poor quality. Approximately 0.002 acre of Wetland A will be filled with 5 cubic yards (cys.) of clean earthen fill.

Wetland B is north of SR 14, inside the circular I-69 southbound entrance ramp (F-8 and F-22). Wetland B is a 0.178-acre PEM1A wetland. Wetland B formed via stormwater runoff from the adjacent roadway and is fed via several small culverts. Wetland B does not have a significant nexus with any jurisdictional waters of the US; therefore, Wetland B should be considered an isolated wetland. Wetland B does not support significant wildlife or aquatic habitat or possess significant hydrologic function; therefore, it should be considered poor quality. This project will not result in any impacts to Wetland B.

Wetland C is north of SR 14, along the I-69 southbound exit ramp (F-9 and F-22). Wetland C is a 0.056-acre PEM1A wetland, located entirely within a RSD. Wetland C did not exhibit significant nexus with any waters of the US; therefore, it should be considered isolated. Wetland C does not support significant wildlife or aquatic habitat or possess significant hydrologic function; therefore, it should be considered poor quality. This project will not result in any impact to Wetland C.

Wetland D is north of SR 14 and west of the southbound lanes of I-69. (F-10 and F-22). Wetland D is a 0.022-acre PEM1A wetland and does not appear to have a significant nexus with any jurisdictional waters of the US. Therefore, Wetland D should be considered an isolated wetland. The wetland appeared to be fed significant polluted storm water via a culvert located under I-69. Wetland D receives polluted run-off from SR 14 and I-69, exhibited low plant species diversity, and is mowed. Therefore, Wetland D should be considered poor quality. This project will not result in any impact to Wetland D.

Wetland E is south of SR 14 and west of I-69. (F-10, F-11, and F-23). Wetland E is a 0.033-acre PEM1A wetland and continues south outside of the project study limits within a RSD. This wetland exhibits a significant nexus with Durnell Ditch via a culvert that carries water from Wetland E underneath the entrance ramp for I-69 southbound to Durnell Ditch, a water of the US. Therefore, Wetland E should be considered a jurisdictional water of the US. Wetland E does not support significant wildlife or aquatic habitat or possess

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significant hydrologic function; therefore, it should be considered poor quality. This project will not result in any impact to Wetland E.

Wetland F was observed south of SR 14 and west of I-69. (F-11, F-12, and F-23). Wetland F is a 0.142-acre PEM1A wetland connected to Wetland E by a drainage pipe. Wetland F exhibits significant nexus with Durnell Ditch, a water of the US. Therefore, Wetland F should be considered a jurisdictional water of the US. Wetland F does not support significant wildlife or aquatic habitat or possess significant hydrologic function; therefore, it should be considered poor quality. This project will not result in any impact to Wetland F.

Wetland G was observed south of SR 14 (F-12, F-13, and F-23). Wetland G is a 0.074-acre PEM1A wetland. Although this wetland has been mapped within the floodplain of Durnell Ditch, the ditch is piped throughout the entirety of the project study limits. Therefore, this wetland did not exhibit a significant nexus with any jurisdictional waters of the US. Wetland G does not support significant wildlife or aquatic habitat or possess significant hydrologic function; therefore, it should be considered poor quality. Approximately 0.003-acre of Wetland G will be filled with 5 cys of clean earthen fill.

A total of approximately 10 cys. of fill will be discharged into wetlands, permanently impacting total of 0.005-acre of wetlands (F-60). Most wetlands on-site have been avoided by minimizing the construction limits. Wetlands within construction limits that are to be avoided will be marked in the field by orange fencing or signs. Wetlands in close proximity to the construction limits will also be marked in the field by orange fencing or signs. These stipulations are included in the Environmental Commitments section of this documents.

This project meets the criteria in the USFWS Interim Policy for programmatic coordination (C-5 to C-7). The USFWS standard recommendations do not mention wetlands.

On August 31, 2017, Metric generated an automatic IDEM Proposed Roadway Construction Projects letter (C-8 to C-11). IDEM recommended a waters investigation and obtaining the appropriate permits in accordance with Sections 401 and 404 of the Clean Water Act.

In a letter, dated September 28, 2017, IDNR, DFW recommended not excavating or placing fill in riparian wetlands (C-16 to C-18). Although the majority of wetlands within the project area will be avoided, some wetland impacts were unavoidable. The appropriate permits shall be obtained, and the avoidance measures discussed above shall be implemented.

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Presence

Impacts

Terrestrial Habitat

Unique or High-Quality Habitat

X

Yes
X

No

Use the remarks

box to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc.).

Remarks: During the August 30, 2017 field reconnaissance, dominant vegetation that Metric observed included lesser poverty rush (*Juncus tenuis*), tall false rye grass (*Schedonorus arundinaceus*), red clover (*Trifolium pratense*), narrow leaf cattail (*Typha angustifolia*), soft-stem club-rush (*Scheonoplectus tabernaemontani*), and Fuller's teasel (*Dipsacus fullonum*) in the herb stratum (F-24 to F-53).

As shown on the 2015 aerial photograph and the site photographs, terrestrial habitat within the project area includes unmanicured grass (B-3 and B-5 to B-12). Based on the design plans, the ground cover to be directly impacted by this project is unmanicured grass (2.14 acres) (B-4 and B-28 to B-38). As greater than 1 acre of soil will be disturbed, a National Pollution Discharge Elimination System (NPDES) General Permit (i.e., Rule 5 permit), pursuant to 327 IAC 15-5, will be required. No trees will be removed as part of this project, as none are present within the construction limits.

This project meets all criteria in the USFWS Interim Policy for programmatic coordination (C-5 to C-7). Recommendations regarding vegetation clearing and reseeded in the Environmental Commitments section of this document.

On August 31, 2017, Metric generated an automatic IDEM Proposed Roadway Construction Projects letter (C-8 to C-11). IDEM recommended minimizing vegetation clearing, reseeded, and obtaining the appropriate permit, pursuant Rule 5. The applicable IDEM recommendations are in the Environmental Commitments section of this document.

In a letter, dated September 28, 2017, IDNR, DFW did not mention unique or high-quality habitat within the project area, but recommended re-seeding disturbed areas (C-16 to C-18). The recommendation is in the Environmental Commitments section of this document.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

Karst

Is the proposed project located within or adjacent to the potential Karst Area of Indiana?

Are karst features located within or adjacent to the footprint of the proposed project?

If yes, will the project impact any of these karst features?

Yes

No

X
X

--

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Use the remarks box to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

Remarks: This project is outside of the designated karst region of Indiana outlined in the Memorandum of Understanding (MOU) between INDOT, IDNR, IDEM, and USFWS (1993).

In the RFI, signed by INDOT ES on January 5, 2018, Metric did not identify karst features such as springs, cave entrances, sinkholes, or sinking stream basins within the 0.5-mile search radius (E-2 and E-7).

In an Environmental Assessment Report, dated August 31, 2017, Indiana Geological Survey (IGS) reported that, within 0.5-mile of the project area, geologic hazards may include a moderate liquefaction potential and a floodway and mineral resources include a high potential for bedrock and a low potential for sand/gravel (C-13 to C-14).

Metric's research and agency coordination indicated that no impact to karst resources is anticipated.

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	Presence	Impacts	
Threatened or Endangered Species		Yes	No
Within the known range of any federal species	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Any critical habitat identified within project area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Is Section 7 formal consultation required for this action?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: The IDNR Allen County Endangered, Threatened and Rare (ETR) Species List was included in the RFI, signed by INDOT ES on January 5, 2018 (E-9 and E-10). The highlighted species on the list include the federal and state identified ETR species. This project is within the range of four federally endangered species of mussels, including Northern riffleshell (*Epioblasma torulosa rangiana*), white catspaw (*Epioblasma obliquata perobliqua*), clubshell (*Pleurobema clava*), and rayed bean (*Villosa fabalis*).

Although they are unlisted, the state of Indiana is within range of the federally threatened Northern long-eared bat (*Myotis septentrionalis*) and federally endangered Indiana bat (*Myotis sodalis*) and portions of the state include the rusty patched bumble bee (*Bombus affinis*). Numerous other ETR are listed on Allen County ETR Species List. However, on August 30, 2017, Metric did not observe the species or their habitats within the project area.

In a letter dated September 28, 2017, IDNR, DFW stated "The Natural Heritage Program's Data had been reviewed. No plant or animal species listed as state or federally threatened, endangered, or rare had been reported to occur within the project vicinity," (C-16 to C-18).

The proposed project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and Northern Long-eared Bat*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration, Federal Transit Administration and USFWS. On February 12, 2018, INDOT ES reviewed the USFWS database, which did not indicate the presence of endangered bat species in or within 0.5 miles of the project area (C-37). On February 12, 2018, project information was submitted through the USFWS Information for Planning and Consultation (IPaC) portal (ecos.fws.gov/ipac/project/list). Allen County is within range of the federally endangered Indiana bat (*Myotis sodalis*) and the federally threatened Northern long-eared bat (*Myotis septentrionalis*) (C-21 to C-26). No critical habitats were present for any species within the project area. On February 12, 2018, Metric generated a consistency letter in IPaC, which stated that this project will have "no effect" on the aforementioned bat species (C-27 to C-36).

This project qualifies for the USFWS Interim Policy; therefore, it is "not likely to adversely affect" any of the other ETR species discussed above (C-5 to C-7).

The requirements under Section 7 of the ESA, as amended, have been met.

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SECTION B – OTHER RESOURCES

Drinking Water Resources	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Wellhead Protection Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Public Water System(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Residential Well(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Source Water Protection Area(s)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Sole Source Aquifer (SSA)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If a SSA is present, answer the following:

	<u>Yes</u>	<u>No</u>
Is the Project in the St. Joseph Aquifer System?	<input type="checkbox"/>	<input type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input type="checkbox"/>	<input type="checkbox"/>
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: The IDEM's Wellhead Proximity Determinator website was accessed on August 31, 2017 by Metric (www.in.gov/idem/cleanwater/pages/wellhead/). The required project location data was provided, and it was determined that the project is not within a Wellhead Protection Area (C-12).

On August 31, 2017, Metric sent an Early Notification letter to the Allen County MS4 coordinator (C-4). No response was received.

Public water lines are not located within the construction limits.

On July 28, 2017, Metric reviewed the IDNR Water Well Record Database (www.in.gov/dnr/water/3595.htm). The nearest well, a Borehole Drilled to Bedrock, is approximately 0.05-mile east of the project area; therefore, no impact is expected.

This project is in Allen County which is not within the boundary of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in Indiana. Therefore, the FHWA/United States Environmental Protection Agency Sole Source Aquifer MOU is not applicable to this project and a groundwater assessment is not required.

No drinking water resources will be impacted by the actions of this project.

Flood Plains	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Longitudinal Encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Project located within a regulated floodplain	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks: The project transversely encroaches upon the regulatory floodplain of Durnell Ditch, as determined from the available Federal Emergency Management Association floodplain maps (E-7 and F-20).

This project falls under INDOT *Categorical Exclusion Manual* (2013) floodplain Category 3: The modifications to drainage structures included in this project will result in an insubstantial change in their capacity to carry flood water. This change could cause a minimal increase in flood heights and flood limits. These minimal increases will not result in any substantial adverse impacts on the natural and beneficial floodplain values; they will not result in substantial change in flood risks or damage; and they do not have substantial potential for interruption or termination of emergency service or emergency routes; therefore, it has been determined that this encroachment is not substantial."

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In a letter, dated September 28, 2017, IDNR, DFW stated that their formal approval would be required for any proposal to construct, excavate, or fill in or on the floodway of Durnell Ditch, unless it qualified for a bridge exemption (C-16 to C-18). Correspondence with IDNR, DFW and INDOT regarding Construction in a Floodway (CIF) permitting is included in the Permits Checklist section of this document and in F-61 to F-67.

	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Farmland			
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006* _____
**If 160 or greater, see CE Manual for guidance.*

See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks: None of the land within construction limits meets the definition of farmland under the Farmland Protection Policy Act. Natural Resources Conservation Service (NRCS) did not provide an early coordination response.

SECTION C – CULTURAL RESOURCES

	<u>Category</u>	<u>Type</u>	<u>INDOT Approval Dates</u>	<u>N/A</u>
Minor Projects PA Clearance	A	2	April 1, 2018	<input type="checkbox"/>

Eligible and/or Listed
Resource Present

Results of Research

Archaeology		<input type="checkbox"/>
NRHP Buildings/Site(s)		<input type="checkbox"/>
NRHP District(s)		<input type="checkbox"/>
NRHP Bridge(s)		<input type="checkbox"/>

Project Effect

No Historic Properties Affected No Adverse Effect Adverse Effect

Documentation
Prepared

Documentation (mark all that apply)

		<u>ES/FHWA Approval Date(s)</u>	<u>SHPO Approval Date(s)</u>
Historic Properties Short Report	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Historic Property Report	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Records Check/ Review	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ia Survey Report	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase Ic Survey Report	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase II Investigation Report	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Archaeological Phase III Data Recovery	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
APE, Eligibility and Effect Determination	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
800.11 Documentation	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Memorandum of Agreement (MOA) **MOA Signature Dates** (List all signatories)

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise

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include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

Remarks: INDOT, acting on behalf of the FHWA, is required to comply with Section 106 of the National Historic Preservation Act of 1966 as amended (Section 106) and its implementing federal regulation, 36 CFR 800. Section 106 and 36 CFR 800 outline a process that requires FHWA and INDOT to evaluate the effects of undertakings on properties that are listed on or eligible for listing on the National Register of Historic Places (NRHP). A Minor Projects Programmatic Agreement (MPPA) between FHWA, INDOT, the Advisory Council on Historic Preservation (ACHP) and the Indiana State Historic Preservation Officer (SHPO) was signed on October 12, 2006. This document streamlines the Section 106 process for certain actions that typically have no effect on properties listed on or eligible for listing on the NRHP.

On April 1, 2018 INDOT Cultural Resources Office concurred that this project can be processed under Categories A-2 of the MPPA (D-1). Category A-2 projects include all work within interchanges and within medians of divided highways in previously disturbed soils (D-2). The Section 106 process has been completed and the FHWA responsibilities for Section 106 have been fulfilled.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement (mark all that apply)

Parks & Other Recreational Land

- Publicly owned park
- Publicly owned recreation area
- Other (school, state/national forest, bikeway, etc.)

Presence

Use

Yes	No

Evaluations

Prepared

- Programmatic Section 4(f)*
- “De minimis” Impact*
- Individual Section 4(f)

FHWA
Approval date

--

Wildlife & Waterfowl Refuges

- National Wildlife Refuge
- National Natural Landmark
- State Wildlife Area
- State Nature Preserve

Presence

Use

Yes	No

Evaluations

Prepared

- Programmatic Section 4(f)*
- “De minimis” Impact*
- Individual Section 4(f)

FHWA
Approval date

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Historic Properties

- Sites eligible and/or listed on the NRHP

Presence

--

Use

Yes	No

Evaluations

Prepared

- Programmatic Section 4(f)*
- “De minimis” Impact*
- Individual Section 4(f)

FHWA
Approval date

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*FHWA approval of the environmental document also serves as approval of any Section 4f Programmatic and/or De minimis evaluation(s) discussed below.

This is page 3 of 6 Project name: Interchange Modification Date: November 29, 2018

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Discuss Programmatic Section 4(f) and "de minimis" Section 4(f) impacts in the remarks box below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, "de minimis" and Individual Section 4(f) evaluations please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks: Section 4(f) of the United States Department of Transportation (USDOT) Act of 1966 prohibits federally funded projects from using land from certain properties unless no feasible and prudent alternative exists. Section 4(f) properties include public parks, recreation areas, and wildlife and waterfowl refuges of national, state, or local significance, as determined by the federal, state, or local officials with jurisdiction thereof; properties listed or determined eligible for inclusion in the NRHP; and archaeological sites that warrant preservation in place.

In the RFI, signed by INDOT ES on January 5, 2018, Metric identified five trails within the 0.5-mile radius, of which one feature, listed as a "Planned" trail, intersected the project area listed (E-2 and E-6).

On August 30, 2017, Metric did not identify any trails or other potential Section 4(f) properties within or adjacent to the project area during a site visit (B-5 to B-12).

In a letter, dated May 8, 2018, NIRCC stated they were not aware of plans to construct the above-referenced "Planned" trail before or during the construction year, but that confirmation from the City of Fort Wayne Department of Public Works would be necessary (C-38). Neither the City of Fort Wayne Parks and Recreation Department nor the Department of Public Works responded to any of the early coordination letters.

Based on the USFWS Refuge List by State, no publicly owned wildlife or waterfowl refuges are within the vicinity of the proposed project (www.fws.gov/refuges/refugeLocatorMaps/Indiana.html).

Metric's research and agency coordination indicated that no trails or other potential 4(f) resources exist within construction limits and none are planned to be built before or during construction. Therefore, no impacts to 4(f) resources are expected and a full Section 4(f) evaluation is not necessary.

Section 6(f) Involvement

Presence

Use

Section 6(f) Property

Yes

No

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks: On December 6, 2017, Metric reviewed the Land and Water Conservation Fund (LWCF) Detailed Listing of Grants for Allen County (I-15 and I-16). No listed LWCF properties were in or adjacent to the project area. Therefore, this project will not involve properties acquired by or improved with the LWCF.

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SECTION E – Air Quality

Conformity Status of the Project

	Yes	No
Is the project in an air quality non-attainment or maintenance area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If YES, then:		
Is the project in the most current MPO TIP?	<input type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input type="checkbox"/>	<input type="checkbox"/>
If the project is NOT exempt from conformity, then:		
Is the project in the Transportation Plan (TP)?	<input type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input type="checkbox"/>

Level of MSAT Analysis required?

Level 1a Level 1b Level 2 Level 3 Level 4 Level 5

Remarks: Allen County is in attainment for all criteria pollutants as reported by IDEM, viewed online on August 31, 2017 (http://www.in.gov/idem/airquality/files/nonattainment_county_list.pdf). This project was incorporated into the 2016-2019 STIP in Amendment 16-01 on August 7, 2015 and into the 2018-2021 STIP in the initial version on July 3, 2017 (H-1 and H-2). This project is located within the Metropolitan Planning Area of NIRCC. The project was incorporated into the NIRCC 2016-2019 TIP in Amendment 16-5 and was carried over into the initial version of the 2018-2021 TIP (H-3 to H-4).

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(c), or exempt under the Clean Air Act conformity rule under 40 CFR 93.126; hot spot analysis and Mobile Source Air Toxics analysis are not required.

SECTION F – NOISE

	Yes	No
Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	No	Yes / Date
ES Review of Noise Analysis	<input type="checkbox"/>	<input type="checkbox"/>

Remarks: This is a Type III project. In accordance with 23 CFR 772 and the *INDOT Traffic Noise Analysis Procedure (2017)*, this action does not require a formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community, and Neighborhood Factors

	Yes	No
Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Does the community have an approved transition plan?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, are steps being made to advance the community's transition plan?	<input type="checkbox"/>	<input type="checkbox"/>
Does the project comply with the transition plan? (explain in the remarks box)	<input checked="" type="checkbox"/>	<input type="checkbox"/>

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Remarks: No permanent or temporary negative impacts to community cohesion, local tax base, or property values will occur as a result of this project.

On August 31, 2017, Metric reviewed the Indiana Association of Fairs website, and no events were listed that could be impacted by this project (www.indianafairsandfestivals.org).

On August 31, 2017, Metric reviewed the Allen County, Indiana Americans with Disabilities Act transition plan (www.allencounty.us/images/human_resources_department/pdfs/ADA_Transition_Plan_-_2016.pdf). Since no pedestrian walkways will be created or modified as a result of this project, it complies with the transition plan.

In a letter, dated May 8, 2018, NIRCC commented about an existing trail system located east and west of the interchange, requested that any interchange modifications at SR 14 and I-69 include trail infrastructure, and recommended coordinating with the City of Fort Wayne Greenways Manager (C-38). The City of Fort Wayne Greenways Manager did not respond to the early coordination letter, dated May 8, 2018 (C-1 to C-3). No trails will be created or modified under the scope of this project; therefore, no impact is expected.

This project will likely address safety issues at the interchange by eliminating weaving conflicts, which will improve safety, thereby impacting the community in a positive manner.

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Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

Indirect impacts are defined in 40 CFR 1508.8 and the INDOT *Categorical Exclusion Manual* (2013) as "Effects that are caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth inducing effects and other effects related to induced changes in the pattern of land use, population density or growth rate and related effects on air and water and other natural systems, including ecosystems."

Cumulative impacts are defined in 40 CFR 1508.7 and the INDOT *Categorical Exclusion Manual* (2013) as "The impact on the environment, which results from the incremental impact of the action when added to other past, present and reasonably foreseeable future actions regardless of what agency (federal or non-federal) or person undertakes such other actions. Cumulative impacts can result from individually minor but collectively significant actions, taking place over a period."

No significant effects have been identified which may induce changes in the patterns of land use, population density or growth rate, or related effects on air and water or other natural systems, including ecosystems. No significant impacts on the environment have been identified which will result from the incremental impact of this action when added to other past, present, and reasonably foreseeable future actions.

There may be some temporary disruption to the community due to construction (e.g., noise and fugitive dust), but these will be eliminated when construction is completed.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public and private utilities, emergency services, religious institutions, airports, public transportation or pedestrian and bicycle facilities? *Discuss how the maintenance of traffic will affect public facilities and services.*

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

I-69 and SR 14 will remain open to traffic during construction. Phased construction with temporary lane and shoulder closures will be utilized, as described in the MOT During Construction section of this document. MOT is not expected to significantly affect public facilities and services. The contractor will be responsible for contacting school districts and emergency services at least 14 days prior to construction.

In the RFI, signed by INDOT ES on January 5, 2018, Metric identified one planned trail and one pipeline within the project area (E-2 and E-6). In a letter, dated October 4, 2017, NIRCC mentioned the pipeline (C-38). Construction limits do not extend to the pipeline. The trail does not currently exist and is not planned to be built before or during construction of this project. Therefore, no impacts to the facilities are expected.

In a letter, dated September 15, 2017, INDOT Office of Aviation stated that an Indiana Tall Structure permit will not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that exceeds 200 ft. above ground level (C-15). No structures of this type will be constructed.

The designer is conducting the utility coordination. No impacts to utilities are anticipated.

No substantial impacts on health or educational facilities, public or private utilities, emergency services, religious institutions, airports, public transportation, or pedestrian or bicycle facilities have been identified.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to EJ populations?

<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

No relocations or permanent right-of-way will be acquired for this project. Therefore, environmental justice analysis is not required.

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Relocation of People, Businesses, or Farms

Will the proposed action result in the relocation of people, businesses, or farms?
 Is a Business Information Survey (BIS) required?
 Is a Conceptual Stage Relocation Study (CSRS) required?
 Has utility relocation coordination been initiated for this project?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the remarks box.

Remarks: No relocations of people, businesses, or farms will be required; therefore, Business Information Survey and Conceptual Stage Relocation Study are not required. No utility relocation is anticipated. The designer is coordinating with utility companies and will continue, as necessary, until project completion.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

Hazardous Materials & Regulated Substances (Mark all that apply)

Red Flag Investigation
 Phase I Environmental Site Assessment (Phase I ESA)
 Phase II Environmental Site Assessment (Phase II ESA)
 Design/Specifications for Remediation required?

Documentation

<input checked="" type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>
<input type="checkbox"/>

No Yes/ Date

ES Review of Investigations		Yes / January 5, 2018
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Include a summary of findings for each investigation.

Remarks:
 In the RFI, signed by INDOT ES on January 5, 2018, Metric identified two Resource Conservation and Recovery Act generators, three leaking underground storage tanks (LUSTs), two underground storage tanks (USTs), and one Institutional Control site within the 0.5-mile search radius (E-3, E-4, and E-8). The nearest facilities, Herdrich Petroleum-Cummings III, a LUST, and Finish Line Car Wash, an UST, are located approximately 0.13-mile east of the project area. Due to their distances from the project area, no impact is expected.

 During the field reconnaissance on August 30, 2017, Metric did not identify any hazardous materials in or near the project area. No additional hazardous materials investigations were deemed to be warranted.

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SECTION I – PERMITS CHECKLIST

Permits (mark all that apply) Likely Required

Army Corps of Engineers (404/Section 10 Permit)

Individual Permit (IP)	
Nationwide Permit (NWP)	X
Regional General Permit (RGP)	
Pre-Construction Notification (PCN)	
Other	
Wetland Mitigation required	
Stream Mitigation required	

IDEM

Section 401 WQC	X
Isolated Wetlands determination	
Rule 5	X
Other	
Wetland Mitigation required	
Stream Mitigation required	

IDNR

Construction in a Floodway	
Navigable Waterway Permit	
Lake Preservation Permit	
Other	
Mitigation Required	
US Coast Guard Section 9 Bridge Permit	
Others (Please discuss in the remarks box below)	

Remarks:

Since approximately 0.005-acre of wetlands will be permanently impacted through discharge of fill, this project must be authorized by the USACE Section 404 and IDEM Section 401 Clean Water Act (CWA) programs.

Since greater than one acre of soil (2.14 acre) will be disturbed, a Rule 5 permit will be required.

On May 21, 2018, INDOT ES preliminarily determined that permits needed were 401/404 NWP's and a Rule 5 permit (F-61 to F-62). INDOT stated that, since all work will occur above the base flood elevation (BFE), a CIF permit will not be needed (F-61).

On May 22, 2018, IDNR, DFW stated "the floodway has infinitely vertical limits." Therefore, any road construction project that crosses a stream with an upstream drainage area of greater than 1 square mile is within the regulatory floodway and requires a CIF permit to construct, excavate, or fill (F-65 to F-66). Metric forwarded the email to INDOT ES and provided more detailed information to IDNR, DFW. On May 23, 2018, IDNR, DFW reiterated, "Any proposal to construct, excavate, or fill in or on the floodway of a stream or other flowing waterbody which has a drainage area greater than one square mile will require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1), unless it qualifies for a general license," (F-65).

This project will involve constructing, excavating, and discharging fill within the floodway of Durnell Ditch, which has an upstream drainage area of 2.302 square miles. The project does not qualify for any general license listed in 312 IAC 10-5-0.3. Since fill of a wetland will occur, criteria 5 in the *INDOT Maintenance Activity Exemption from the Flood Control and Navigable Waterways Act (1997)* has not been met. Therefore, the project does not qualify for this exemption.

On May 23, 2018, INDOT ES stated that the project is not anticipated to require a CIF permit (F-63).

It will be the responsibility of the designer to submit plans to INDOT ES to process permit applications.

Indiana Department of Transportation

County Allen Route I-69 at SR 14 Des. No. 1401828

SECTION J – ENVIRONMENTAL COMMITMENTS

The following information should be provided below: List all commitments, name of agency/organization requesting the commitment(s) and indicating which are firm and which are for further consideration. The commitments should be numbered.

Remarks:

Firm:

1. The project sponsor will be responsible for contacting school districts and emergency services at least 2 weeks prior to construction. (INDOT)
2. If the scope of work or permanent or temporary right of way amounts change, INDOT ES and the INDOT Fort Wayne District shall be contacted immediately. (INDOT)
3. If wetland impacts increase beyond 0.100 acre, INDOT Environmental Services shall be contacted (INDOT)
4. If a spill occurs or contaminated soils or water are encountered during construction, appropriate personal protective equipment (PPE) should be used. Contaminated materials will need to be properly handled by trained personnel and disposed in accordance with current regulations. IDEM should be notified through the spill line at (888) 233-7745 within 24 hours of discovery of a release from a UST system and within two (2) hours of discovery of a spill. (INDOT)
5. If any human remains are encountered, work shall cease in the immediate area and the human remains left undisturbed. INDOT and FHWA will contact the county coroner and law enforcement officials immediately, and the discovery must be reported to the SHPO within two (2) business days. The discovery must be treated in accordance with IC 14-21-1 and 312 IAC 22. If the remains are determined to be Native American, FHWA will notify the appropriate federally recognized Indian Tribes. Work at the site shall not resume until a plan for the treatment of the human remains is developed and approved in consultation with the SHPO and any appropriate consulting parties. The plan will comply with IC 14-21-1, 312 IAC 22, the current Guidebook for Indiana Historic Sites and Structures Inventory – Archaeological Sites, and all other appropriate federal and state guidelines, statutes, rules, and regulations. (IDNR, SHPO)
6. No temporary lighting shall be used. If temporary lighting is deemed necessary, INDOT Environmental Services shall be contacted. (INDOT)
7. No tree removal shall occur. If tree removal is deemed necessary, INDOT Environmental Services shall be contacted. (INDOT)
8. Wetlands to be avoided within construction limits shall be marked in the field by orange fencing or signs. (INDOT)
9. Wetlands in close proximity to the construction limits shall be marked in the field by orange fencing or signs. (INDOT)

For Consideration:

10. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue) and legumes as soon as possible upon completion. Low endophyte tall fescue may be used in the ditch bottom and side slopes only. (IDNR, DFW; IDEM; USFWS)
11. Appropriately designed measures for controlling erosion sediment must be implemented to prevent sediment from leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized. (IDNR, DFW; IDEM; USFWS)
12. Do not excavate or place fill in any riparian wetland (IDNR, DFW)
13. Do not clear trees or understory vegetation outside the boundaries. (USFWS)
14. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications. (USFWS)
15. Revegetate all disturbed soil areas immediately upon project completion. (USFWS)
16. Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. Dirt tracked onto paved roads from unpaved areas should be minimized. (IDEM)
17. Stabilize all disturbed areas upon completion of construction activities. (IDEM)
18. Wastes and unused building materials shall be managed and disposed of in accordance with all applicable statutes and regulations. (IDEM)
19. A stable construction site access shall be provided at all points of construction traffic ingress and egress to the project site. (IDEM)
20. Public or private roadways shall be kept cleared of accumulated sediment that is a result of run-off or tracking. (IDEM)

This is page 1 of 1 Project name: Interchange Modification Date: November 29, 2018

Indiana Department of Transportation

County Allen Route I-69 at SR 14 Des. No. 1401828

SECTION K – EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received. INDOT and FHWA are automatically considered early coordination participants and should only be listed if a response is received.

Remarks:

Resource Agency	Date Sent	Date Response
NRCS	August 31, 2017	No Response
IGS	August 31, 2017	August 31, 2017
INDOT Office of Aviation	August 31, 2017	September 15, 2017
National Parks Service	August 31, 2017	No Response
IDNR, DFW	August 31, 2017	September 28, 2017
US Department of Housing and Urban Development	August 31, 2017	No Response
USFWS	August 31, 2017	September 13, 2017
Allen County Surveyor	August 31, 2017	No Response
INDOT Public Hearings	August 31, 2017	No Response
USACE	August 31, 2017	No Response
Allen County Highway Department	August 31, 2017	No Response
Allen County Commissioners	August 31, 2017	No Response
IDEM Proposed Roadway Construction Projects Letter	August 31, 2017	August 31, 2017
IDEM Wellhead Proximity Determinator	August 31, 2017	August 31, 2017
NIRCC	August 31, 2017; April 12, 2018	October 4, 2017; May 8, 2018
City of Fort Wayne, Department of Parks and Recreation		No Response
City of Fort Wayne, Department of Public Works	May 8, 2018	No Response

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APPENDIX A: INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 ¹
Section 106	Falls within guidelines of Minor Projects PA	"No Historic Properties Affected"	"No Adverse Effect"	-	"Adverse Effect" Or Historic Bridge involvement ²
Stream Impacts	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	Individual 404 Permit
Wetland Impacts	No adverse impacts to wetlands	< 0.1 acre	-	< 1 acre	≥ 1 acre
Right-of-way³	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
Relocations	None	-	-	< 5	≥ 5
Threatened/Endangered Species (Species Specific Programmatic for Indiana bat & northern long eared bat)	"No Effect", "Not likely to Adversely Affect" (Without AMMs ⁴ or with AMMs required for all projects ⁵)	"Not likely to Adversely Affect" (With any other AMMs)	-	"Likely to Adversely Affect"	Project does not fall under Species Specific Programmatic
Threatened/Endangered Species (Any other species)	Falls within guidelines of USFWS 2013 Interim Policy	"No Effect", "Not likely to Adversely Affect"	-	-	"Likely to Adversely Affect"
Environmental Justice	No disproportionately high and adverse impacts	-	-	-	Potential ⁶
Sole Source Aquifer	Detailed Assessment Not Required	-	-	-	Detailed Assessment
Floodplain	No Substantial Impacts	-	-	-	Substantial Impacts
Coastal Zone Consistency	Consistent	-	-	-	Not Consistent
National Wild and Scenic River	Not Present	-	-	-	Present
New Alignment	None	-	-	-	Any
Section 4(f) Impacts	None	-	-	-	Any
Section 6(f) Impacts	None	-	-	-	Any
Added Through Lane	None	-	-	-	Any
Permanent Traffic Alteration	None	-	-	-	Any
Coast Guard Permit	None	-	-	-	Any
Noise Analysis Required	No	-	-	-	Yes
Air Quality Analysis Required	No	-	-	-	Yes ⁷
Approval Level	Concurrence by INDOT District Environmental or Environmental Services	Yes	Yes	Yes	Yes
<ul style="list-style-type: none"> • District Env. Supervisor • Env. Services Division • FHWA 				Yes	Yes

¹Coordinate with INDOT Environmental Services. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

²Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

³Permanent and/or temporary right-of-way.

⁴AMMs = Avoidance and Mitigation Measures.

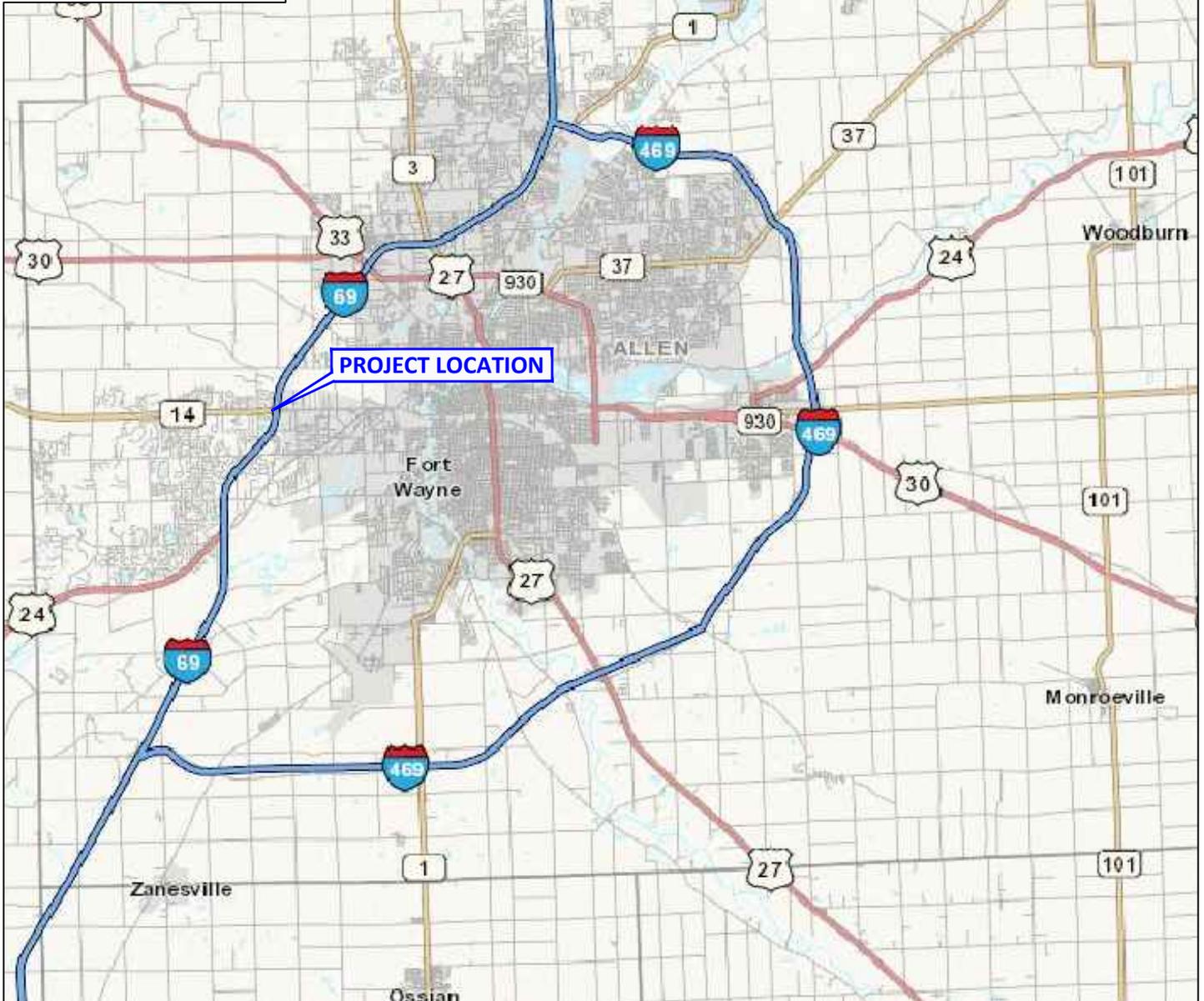
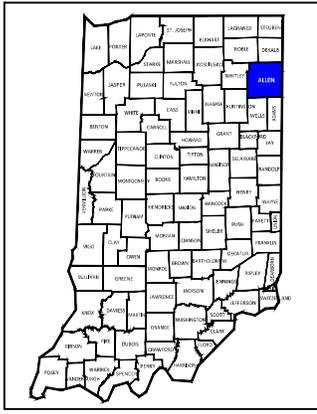
⁵AMMs determined by the IPAC decision key to be needed that are listed in the USFWS *User's Guide for the Range-wide Programmatic Consultation for Indiana bat and Northern long-eared bat* as "required for all projects".

⁶Potential for causing a disproportionately high and adverse impact.

⁷Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

*Substantial public or agency controversy may require a higher-level NEPA document.

APPENDIX B: Graphics



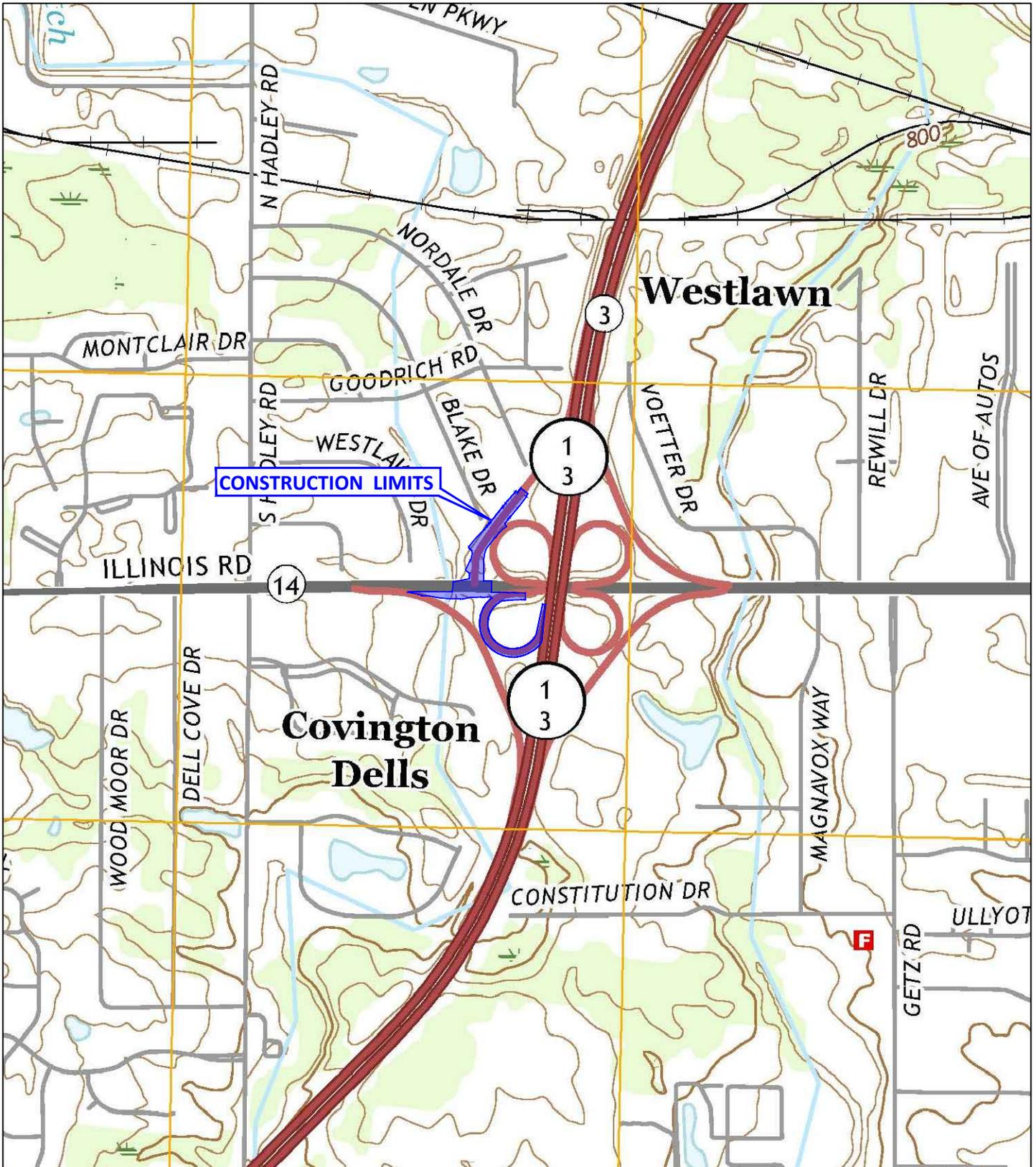
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Location Map
 I-69 at S.R. 14
 Interchange Modifications
 Aboite Township, Allen County, Indiana
 Des. No. 1401828

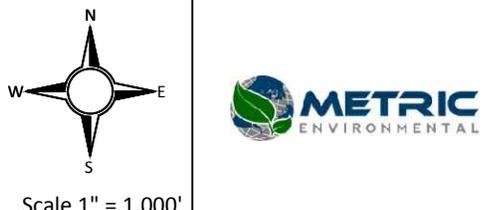
All locations approximate

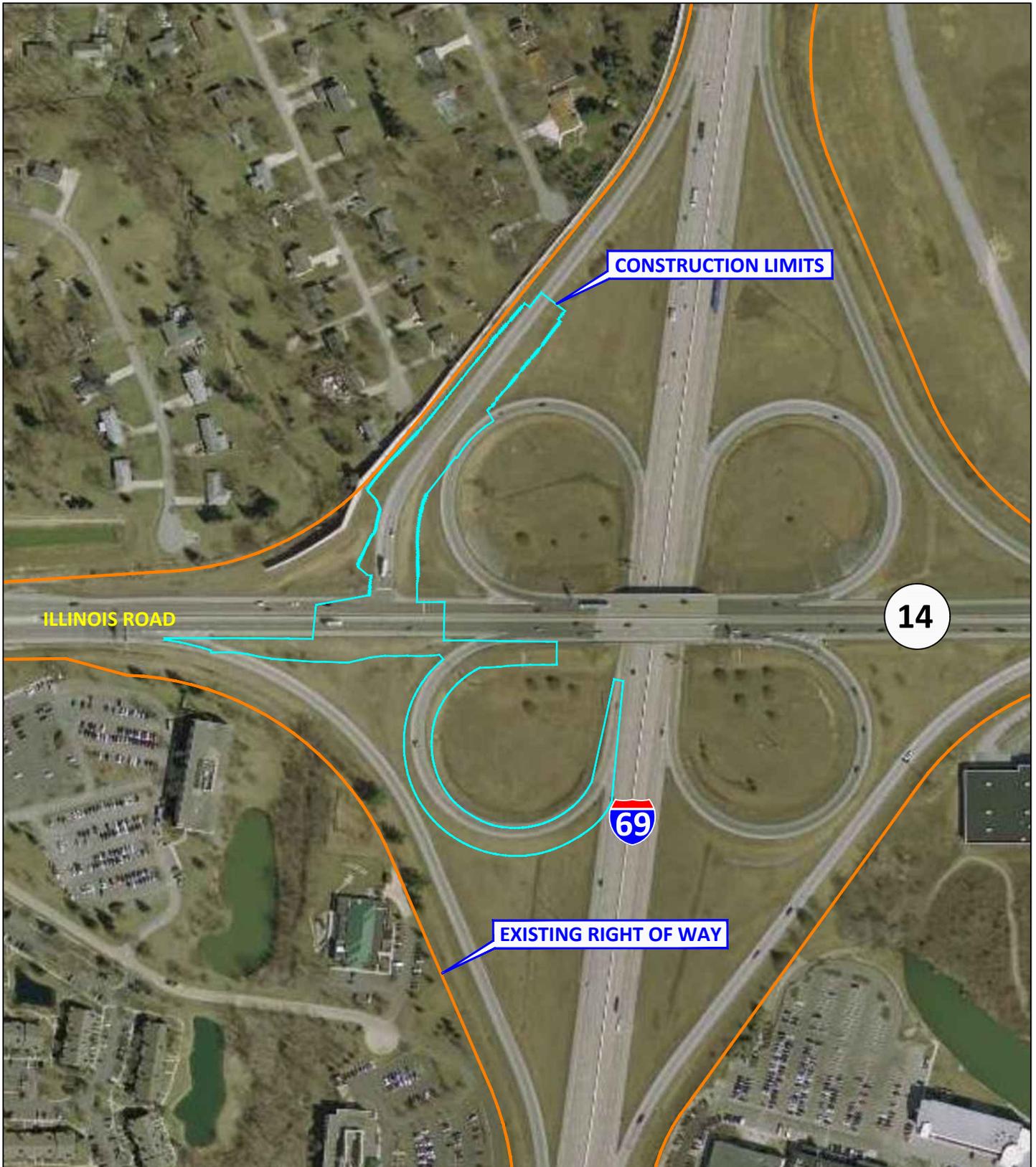
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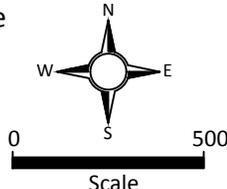


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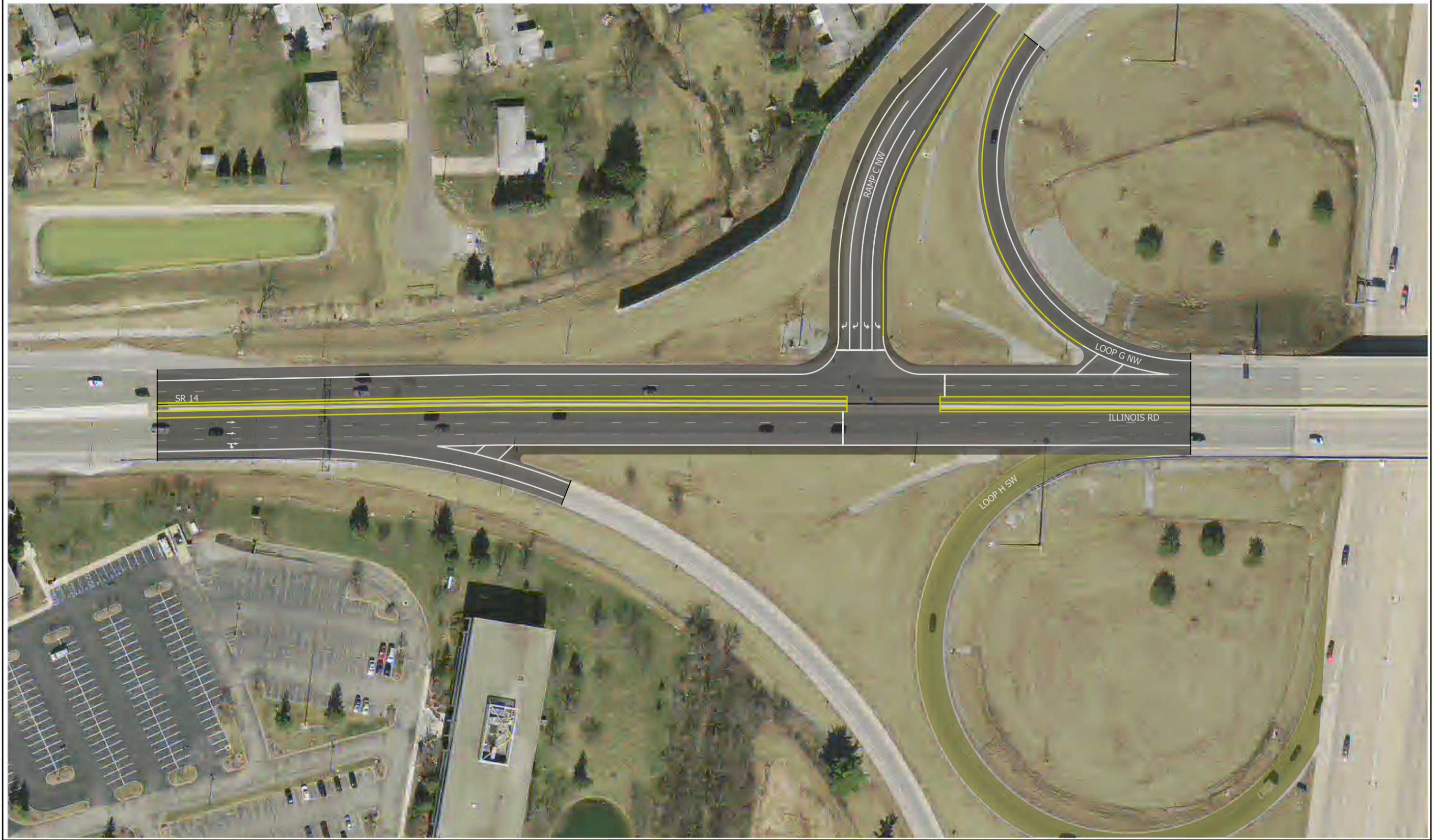
<p>USGS Topographic Map I-69 at S.R. 14 Interchange Modifications Aboite Township, Allen County, Indiana Des. No. 1401828</p>	<p>All locations approximate Base map: 2016 Fort Wayne West, IN 7.5 Minute Quadrangle</p>	 <p>Scale 1" = 1,000'</p>
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Source: <http://maps.indy.gov/MapIndy/>

<p>2015 Aerial Photograph I-69 at S.R. 14 Interchange Modifications Aboite Township, Allen County, Indiana Des. No. 1401828</p>	<p>All locations approximate</p>  <p>Scale</p>	
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S:\COLL\4000-1099\4060\273\Micros\Closed Loop.pbn.dgn
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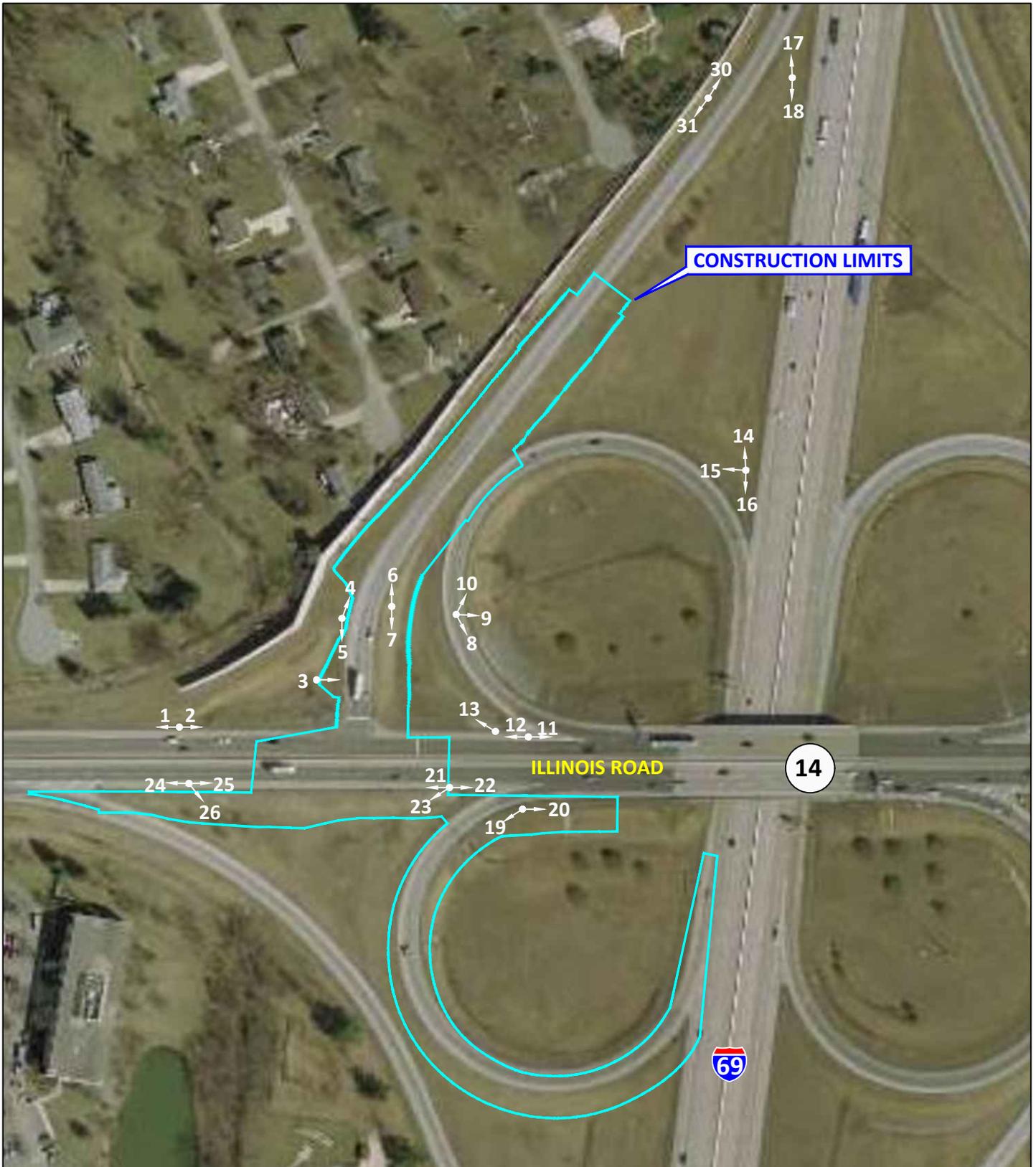
'Closed Loop' Alternative (Preferred Alternative) Illustration
 Created by Strand Associates

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DESIGNED: _____	DRAWN: _____	
CHECKED: _____	CHECKED: _____	

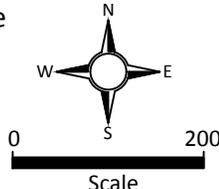
INDIANA
 DEPARTMENT OF TRANSPORTATION

 CLOSED LOOP
 SB RAMP TERMINAL

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE NO.
VERTICAL SCALE	DESIGNATION NO.
SURVEY BOOK NO.	SHEETS
CONTRACT NO.	\$PAGE_NUM\$ of PROJECT NO.



Source: <http://maps.indy.gov/MapIndy/>

<p>Photograph Location Map I-69 at S.R. 14 Interchange Modifications Aboite Township, Allen County, Indiana Des. No. 1401828</p>	<p>All locations approximate</p>  <p>Scale</p>	
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1. View of S.R. 14 West from western project boundary, looking west.



2. View of S.R. 14 West from western project boundary, looking east.



3. View of drainage pipe north of S.R. 14 West and west of I-69 South off ramp looking east.



4. View of I-69 South exit ramp right-of-way, looking northeast.

Site Photographs
I-69 at S.R. 14
Interchange Modifications
Aboite Township, Allen County, Indiana
Des. No. 1401828





5. View of I-69 South exit ramp right-of-way, looking south.



6. View of I-69 South off ramp and road side ditch (RSD) 1, looking north.



7. View of I-69 South exit ramp right-of-way and RSD 1 outlet to Wetland A, looking south.



8. View of I-69 South entrance ramp right-of-way, looking southeast.

Site Photographs
I-69 at S.R. 14
Interchange Modifications
Aboite Township, Allen County, Indiana
Des. No. 1401828





9. View of I-69 South entrance ramp right-of-way, looking east at Wetland B.



10. View of I-69 South entrance ramp right-of-way looking northeast at Wetland.



11. View of I-69 South entrance ramp right-of-way, looking northeast at Wetland.



12. View of S.R. 14 West at I-69 South entrance ramp, looking west.

Site Photographs
I-69 at S.R. 14
Interchange Modifications
Aboite Township, Allen County, Indiana
Des. No. 1401828





13. View of riprap north of SR 14, looking northwest.



14. View of I-69 South right-of-way, looking north.



15. View of I-69 South entrance ramp right-of-way looking east.



16. View of I-69 South, looking south.

Site Photographs
I-69 at S.R. 14
Interchange Modifications
Aboite Township, Allen County, Indiana
Des. No. 1401828





17. View of I-69 South exit ramp looking north.



18. View of I-69 South right-of-way and Wetland D looking south.



19. View of I-69 South exit ramp to S.R. 15 east, looking southwest.



20. View of S.R. 14 East right-of-way looking east.

Site Photographs
I-69 at S.R. 14
Interchange Modifications
Aboite Township, Allen County, Indiana
Des. No. 1401828





21. View of S.R. 14 East right-of-way, looking west.



22. View of S.R. 14 East right-of-way, looking east.



23. View of rip-rap drainage feature south of S.R. 14 East looking southwest.



24. View of S.R. 14 East from western project boundary, looking west.

Site Photographs
I-69 at S.R. 14
Interchange Modifications
Aboite Township, Allen County, Indiana
Des. No. 1401828





25. View of S.R. 14 East from western project boundary, looking east.



26. View of I-69 South right-of-way and Wetland G, looking southeast.

Site Photographs
I-69 at S.R. 14
Interchange Modifications
Aboite Township, Allen County, Indiana
Des. No. 1401828



PROJECT	DESIGNATION
1401828	1401828
CONTRACT	BRIDGE FILE NO.
R-38565	N/A

INDIANA DEPARTMENT OF TRANSPORTATION



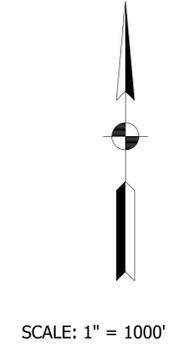
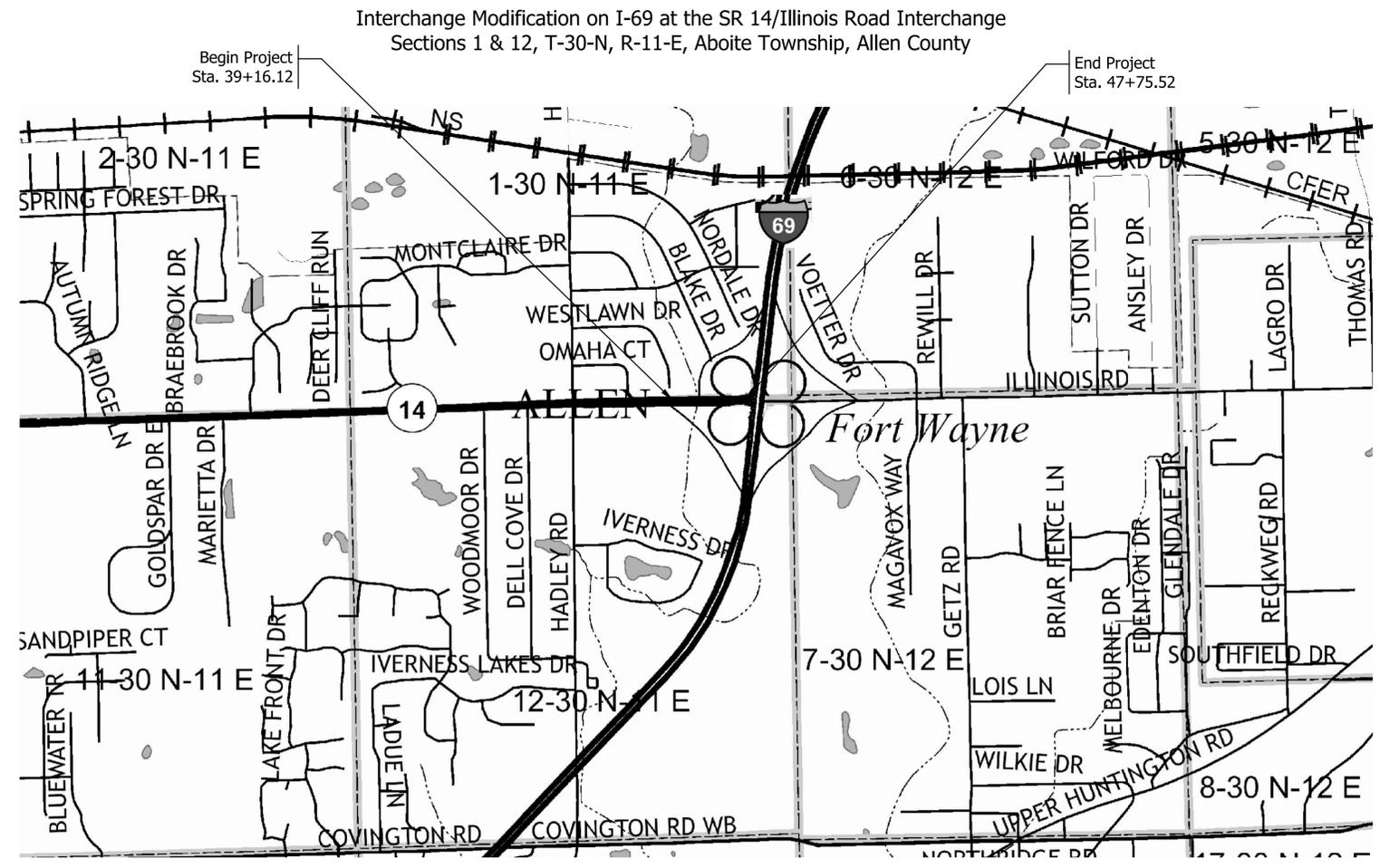
ROAD PLANS

TRAFFIC DATA		
	SR 14/ILLINOIS RD	RAMP C NW
A.A.D.T. (2015)	30,749 V.P.D.	13,608 V.P.D.
A.A.D.T. (2040)	39,205 V.P.D.	15,990 V.P.D.
D.H.V. (2040)	3,470 V.P.H.	1,638 V.P.H.
DIRECTIONAL DISTRIBUTION	49.9 %	100 %
TRUCKS	4 % A.A.D.T.	4 % A.A.D.T.
	4 % D.H.V.	4 % D.H.V.

DESIGN DATA		
DESIGN SPEED	45 M.P.H.	45 M.P.H.
PROJECT DESIGN CRITERIA	3R (NON-FREEWAY)	3R (FREEWAY)
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL	INTERSTATE
RURAL/URBAN	URBAN	URBAN
TERRAIN	LEVEL	LEVEL
ACCESS CONTROL	NONE	FULL

KIN PROJECT INFORMATION	
DESIGNATION	PROJECT INFORMATION
1600115	Preventative Maintenance Overlay of SR 14

ROUTE: I-69 @ SR 14 FROM: RP 305+18 TO: RP 305+37
PROJECT NO. 1401828 P.E. 1401828 R/W 1401828 CONST. 1401828



LATITUDE: 41° 04' 28" N	LONGITUDE: 85° 13' 42" W
BRIDGE LENGTH: N/A	MI.
ROADWAY LENGTH: 0.163 mi.	MI.
TOTAL LENGTH: 0.163 mi.	MI.
MAX. GRADE: 3.0%	%
HUC: 05120101100020	

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2018
TO BE USED WITH THESE PLANS

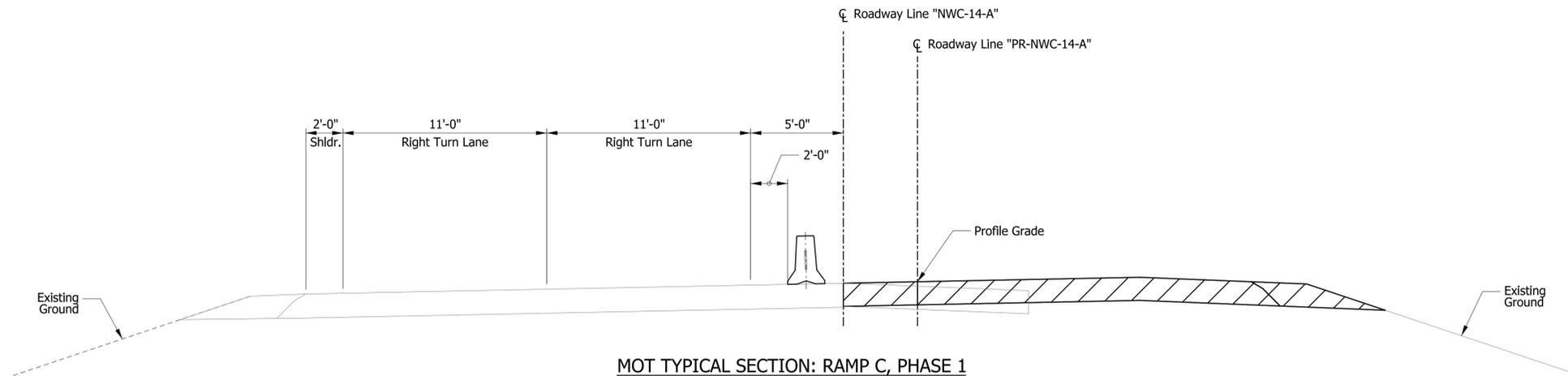
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PLANS PREPARED BY:	STRAND ASSOCIATES, INC.	(812)372-9911
	629 WASHINGTON ST., COLUMBUS, IN 47201	PHONE NUMBER
CERTIFIED BY:		DATE
APPROVED FOR LETTING:	INDIANA DEPARTMENT OF TRANSPORTATION	DATE

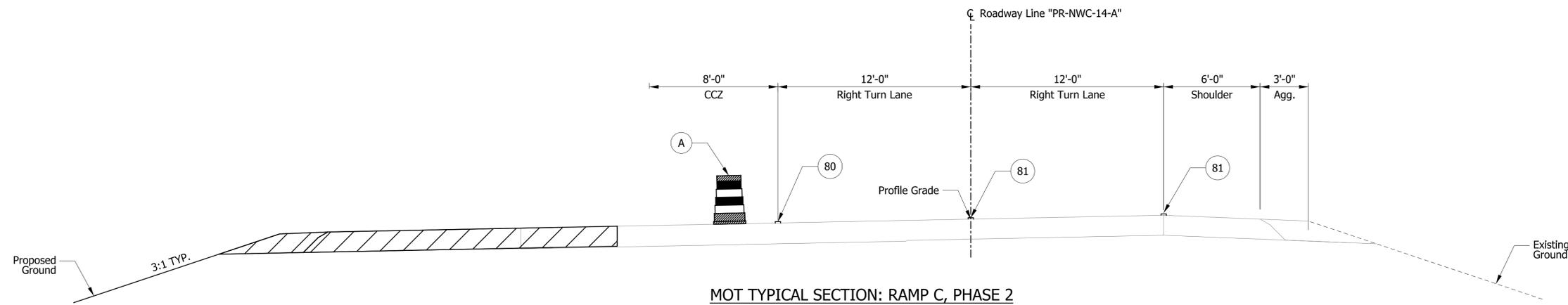
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N/A	
DESIGNATION	
1401828	
SURVEY BOOK	SHEETS
	1 of 48
CONTRACT	PROJECT
R-38565	1401828

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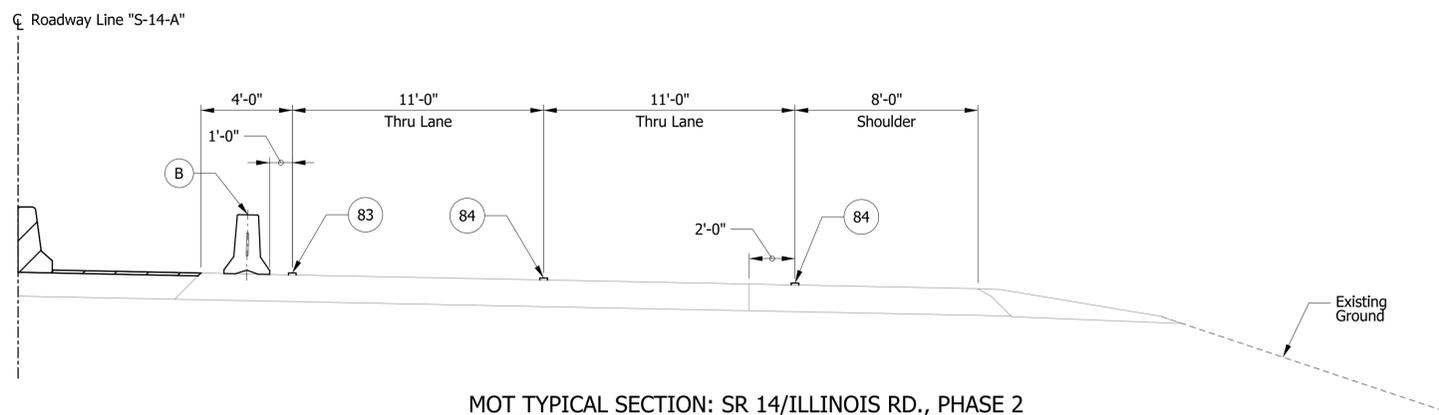
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MOT TYPICAL SECTION: RAMP C, PHASE 2

Sta. 7+13.00 "PR-NWC-14-A" to Sta. 15+01.87 "PR-NWC-14-A"



MOT TYPICAL SECTION: SR 14/ILLINOIS RD., PHASE 2

Sta. 41+97 "S-14-A" to Sta. 47+23 "S-14-A" EB & WB

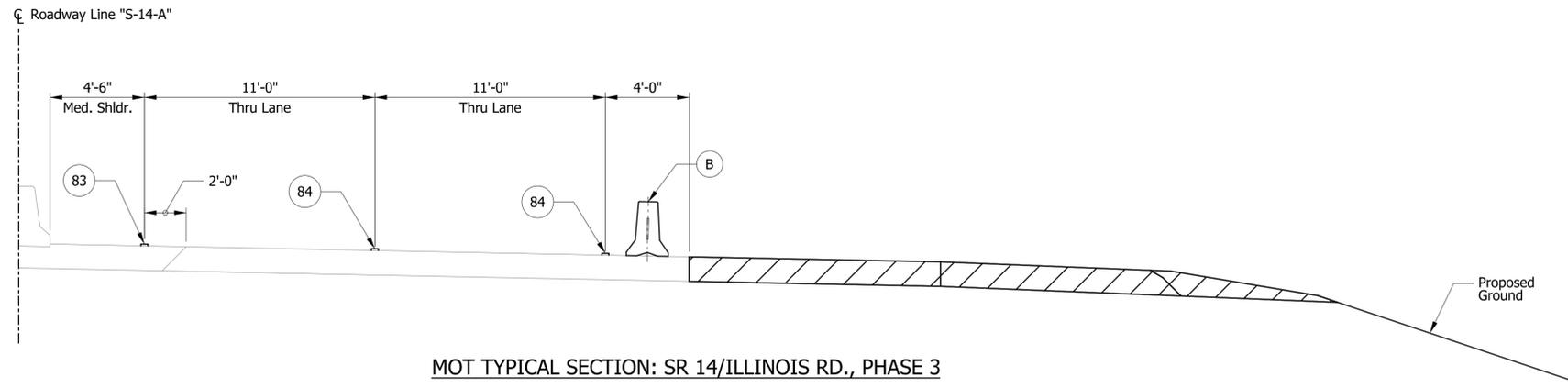
- LEGEND**
- 80 Temporary Pavement Marking, Yellow, 4"
 - 81 Temporary Pavement Marking, White, 4"
 - 83 Temporary Pavement Marking, Removable, Yellow, 4"
 - 84 Temporary Pavement Marking, Removable, White, 4"
 - A Construction Drum
 - B Temporary Barrier Wall, Type 2
 - Construction Area

NOTE:
 Mill and overlay on Ramp C shall be completed with surface course in Phase 2.

RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA DEPARTMENT OF TRANSPORTATION
MOT TYPICAL CROSS SECTIONS
PHASES 1 & 2

HORIZONTAL SCALE	BRIDGE FILE NO.
1/4" = 1'-0"	NA
VERTICAL SCALE	DESIGNATION NO.
	1401828
SURVEY BOOK NO.	SHEETS
	3 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828



MOT TYPICAL SECTION: SR 14/ILLINOIS RD., PHASE 3

Sta. 40+19.13 "S-14-A" to Sta. 47+75.52 "S-14-A" EB

- LEGEND**
- 80 Temporary Pavement Marking, Yellow, 4"
 - 81 Temporary Pavement Marking, White, 4"
 - 83 Temporary Pavement Marking, Removable, Yellow, 4"
 - 84 Temporary Pavement Marking, Removable, White, 4"
 - A Construction Drum
 - B Temporary Barrier Wall, Type 2
 - Construction Area

NOTE:
Mill and overlay on Ramp C shall be completed with surface course in Phase 2.

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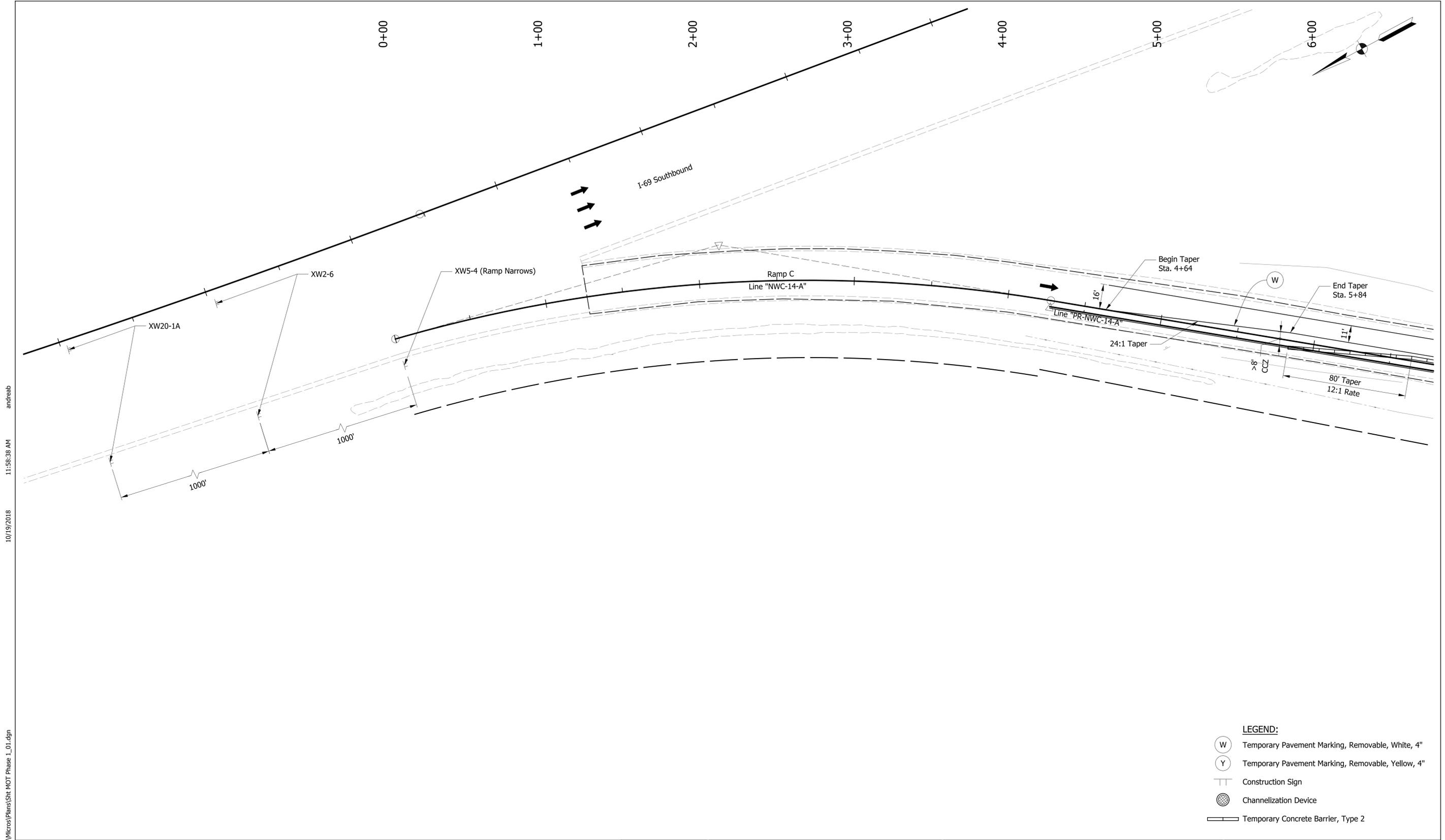
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RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**MOT TYPICAL CROSS SECTIONS
I-69 AT SR 14/ILLINOIS RD**

HORIZONTAL SCALE 1/4" = 1'-0"	BRIDGE FILE NO. NA
VERTICAL SCALE	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 4 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828



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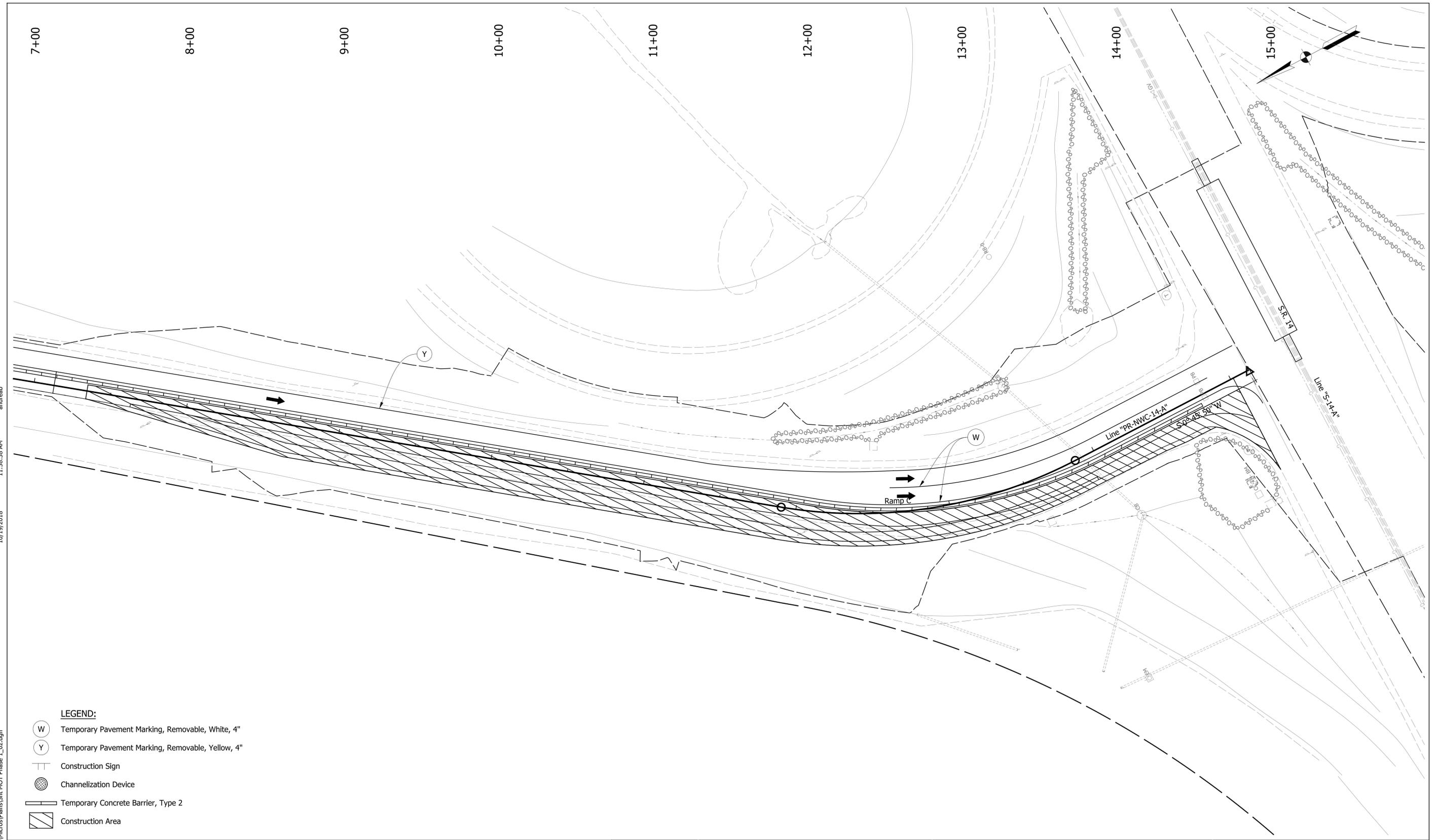
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- (Y) Temporary Pavement Marking, Removable, Yellow, 4"
- Construction Sign
- Channelization Device
- Temporary Concrete Barrier, Type 2

	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 30' VERTICAL SCALE N/A	BRIDGE FILE NO. N/A DESIGNATION NO. 1401828
	DESIGNED: ALB CHECKED: MAR	DRAWN: ACB CHECKED: MAR	MAINTENANCE OF TRAFFIC PHASE 1	SURVEY BOOK NO. CONTRACT NO. R-38565
				SHEETS 5 of 48 PROJECT NO. 1401828

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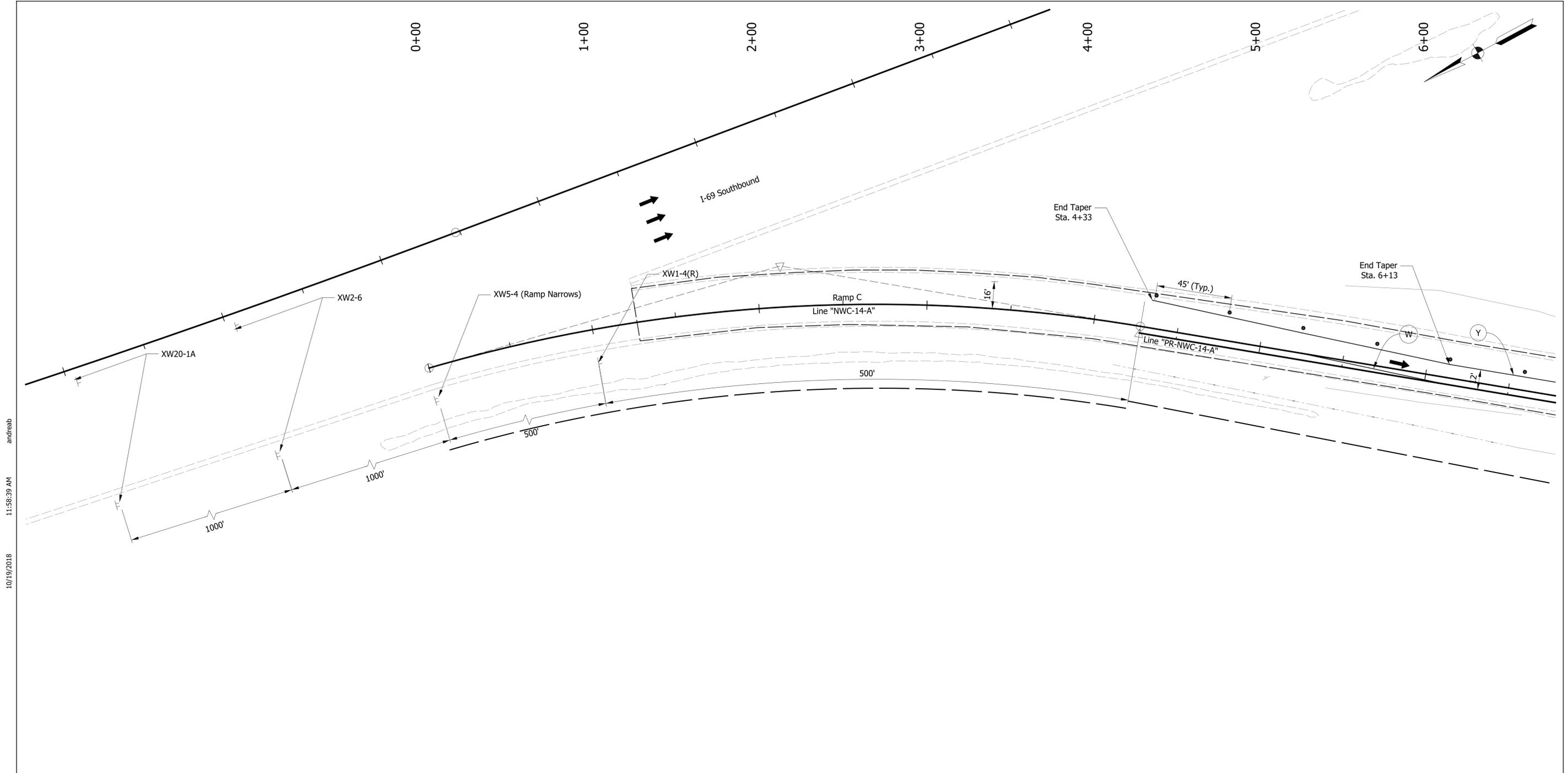
- LEGEND:**
- W Temporary Pavement Marking, Removable, White, 4"
 - Y Temporary Pavement Marking, Removable, Yellow, 4"
 - Construction Sign
 - Channelization Device
 - Temporary Concrete Barrier, Type 2
 - Construction Area

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PHASE 1

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION NO.
N/A	1401828
SURVEY BOOK NO.	SHEETS
	6 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828



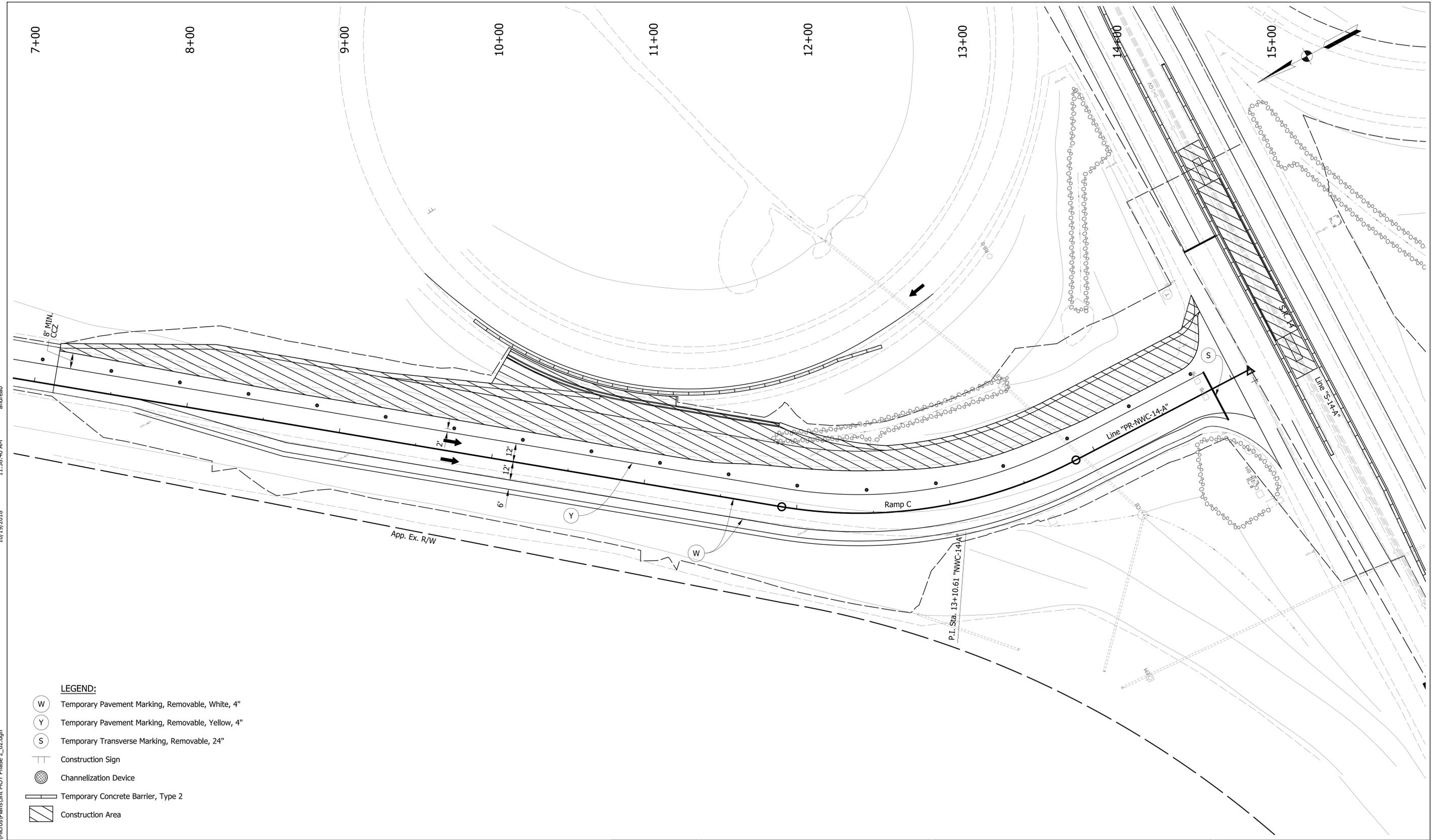
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LEGEND:

	Temporary Pavement Marking, Removable, White, 4"
	Temporary Pavement Marking, Removable, Yellow, 4"
	Construction Sign
	Channelization Device
	Temporary Concrete Barrier, Type 2

	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 30' VERTICAL SCALE N/A	BRIDGE FILE NO. N/A DESIGNATION NO. 1401828
	DESIGNED: ALB DRAWN: ACB CHECKED: MAR CHECKED: MAR	MAINTENANCE OF TRAFFIC PHASE 2 - RAMP C	SURVEY BOOK NO. N/A	SHEETS 7 of 48 CONTRACT NO. R-38565 PROJECT NO. 1401828

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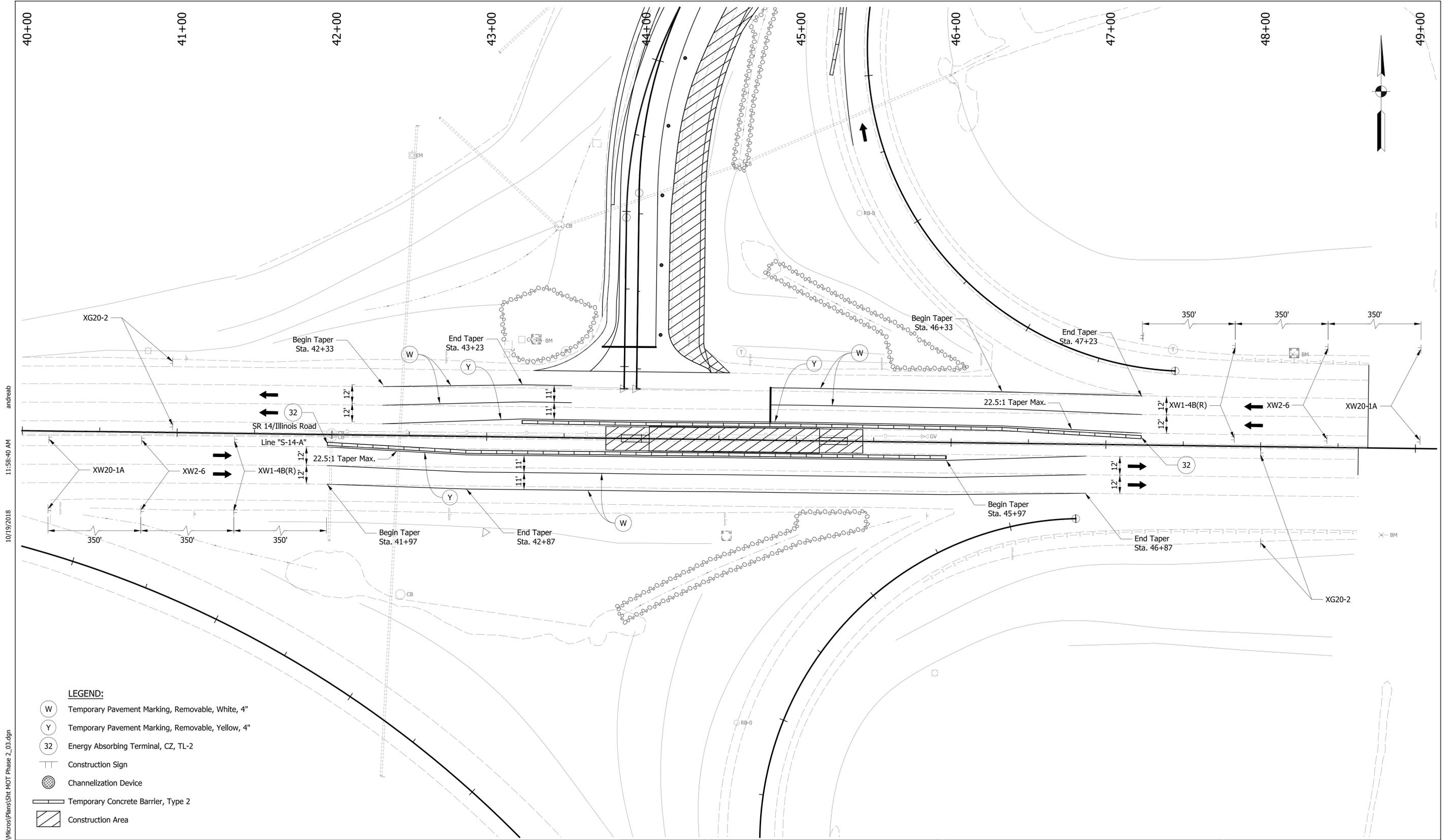
- LEGEND:**
- W Temporary Pavement Marking, Removable, White, 4"
 - Y Temporary Pavement Marking, Removable, Yellow, 4"
 - S Temporary Transverse Marking, Removable, 24"
 - Construction Sign
 - Channelization Device
 - Temporary Concrete Barrier, Type 2
 - Construction Area

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB
CHECKED: MAR	CHECKED: MAR

INDIANA DEPARTMENT OF TRANSPORTATION

**MAINTENANCE OF TRAFFIC
PHASE 2 - RAMP C**

HORIZONTAL SCALE 1" = 30'	BRIDGE FILE NO. N/A
VERTICAL SCALE N/A	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 8 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828



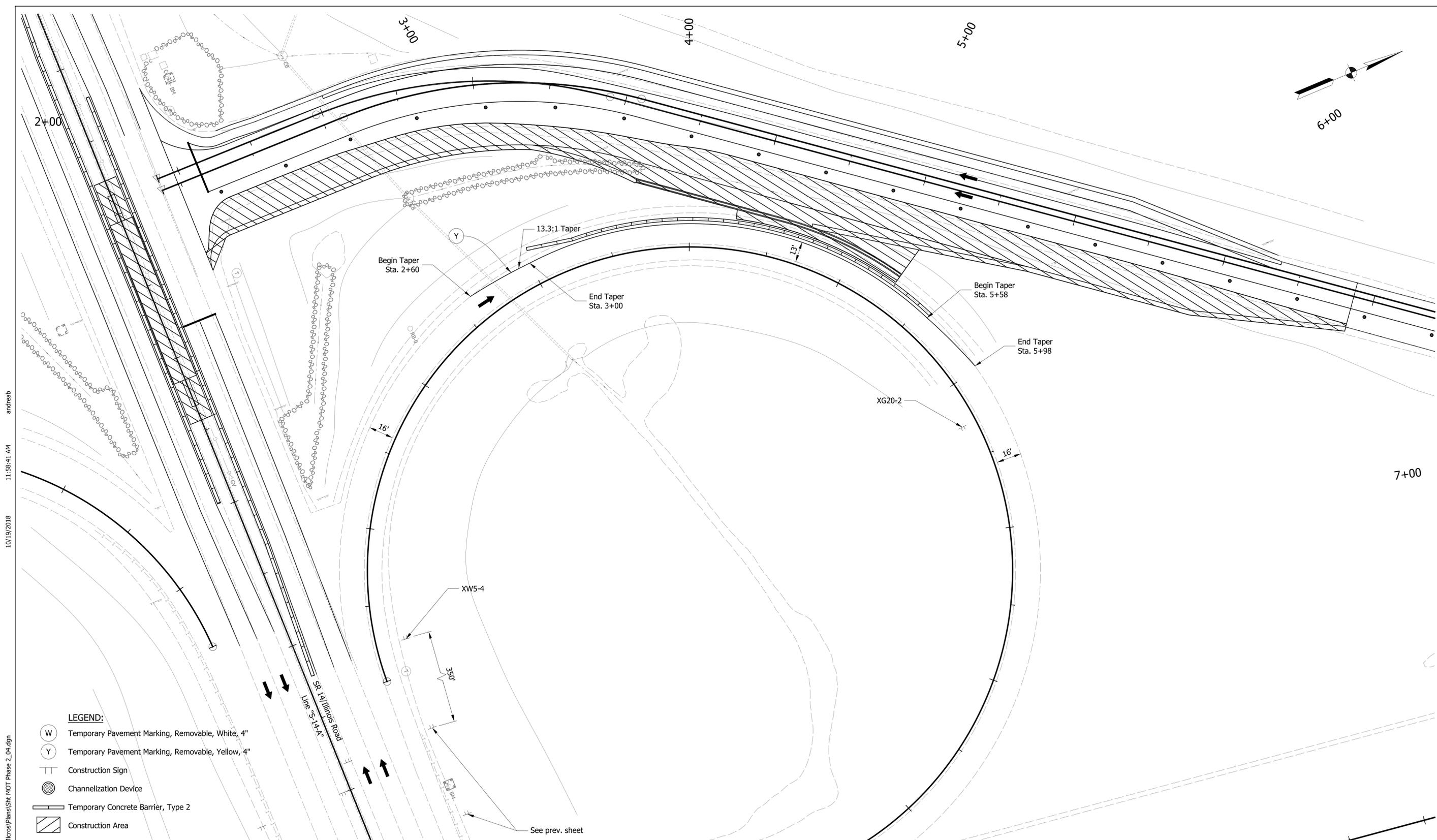
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- LEGEND:**
- W Temporary Pavement Marking, Removable, White, 4"
 - Y Temporary Pavement Marking, Removable, Yellow, 4"
 - 32 Energy Absorbing Terminal, CZ, TL-2
 - Construction Sign
 - Channelization Device
 - Temporary Concrete Barrier, Type 2
 - Construction Area

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____ DATE _____
DESIGNED: ALB	DRAWN: ACB
CHECKED: MAR	CHECKED: MAR

INDIANA DEPARTMENT OF TRANSPORTATION
MAINTENANCE OF TRAFFIC
PHASE 2 - SR 14

HORIZONTAL SCALE 1" = 30'	BRIDGE FILE NO. N/A
VERTICAL SCALE	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 9 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828



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- LEGEND:**
- (W) Temporary Pavement Marking, Removable, White, 4"
 - (Y) Temporary Pavement Marking, Removable, Yellow, 4"
 - Construction Sign
 - ⊗ Channelization Device
 - ▬ Temporary Concrete Barrier, Type 2
 - ▨ Construction Area

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB
CHECKED: MAR	CHECKED: MAR

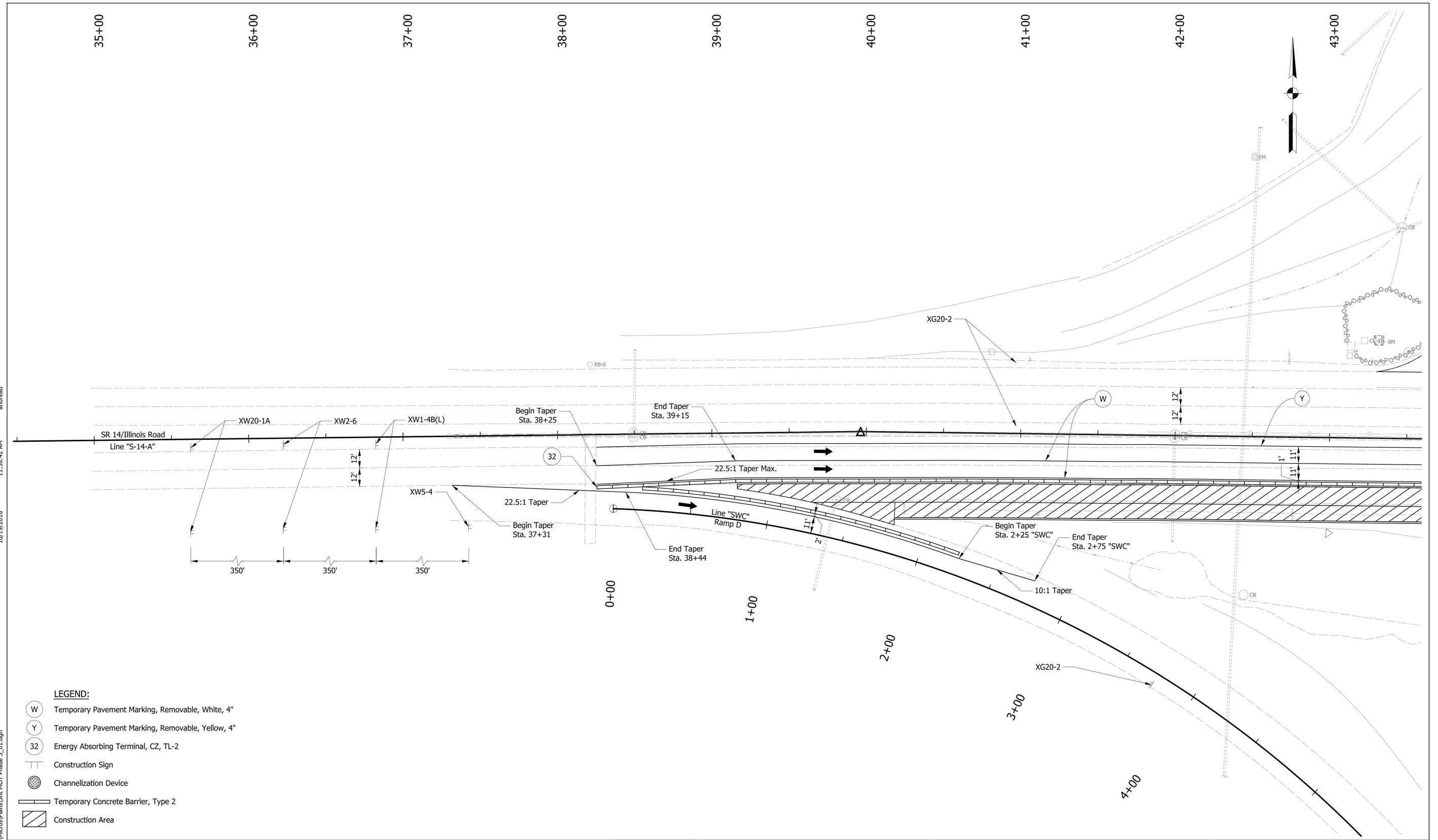
INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 2 - RAMP G

HORIZONTAL SCALE 1" = 30'	BRIDGE FILE NO. N/A
VERTICAL SCALE	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 10 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828

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- LEGEND:**
- (W) Temporary Pavement Marking, Removable, White, 4"
 - (Y) Temporary Pavement Marking, Removable, Yellow, 4"
 - (32) Energy Absorbing Terminal, CZ, TL-2
 - T Construction Sign
 - Channelization Device
 - Temporary Concrete Barrier, Type 2
 - Construction Area

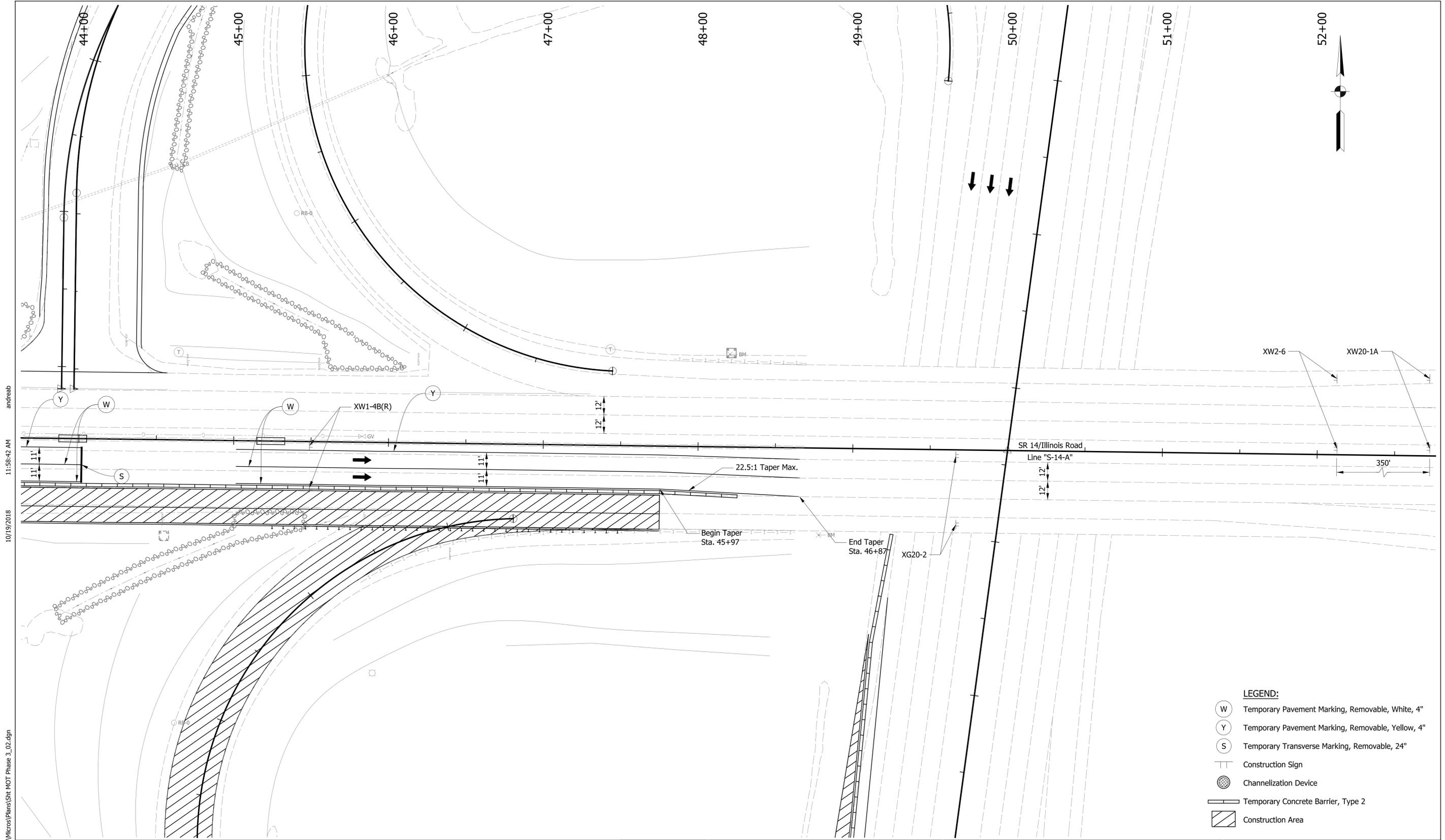
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC

PHASE 3 - SR 14

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION NO.
N/A	1401828
SURVEY BOOK NO.	SHEETS
	11 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828



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LEGEND:

- W Temporary Pavement Marking, Removable, White, 4"
- Y Temporary Pavement Marking, Removable, Yellow, 4"
- S Temporary Transverse Marking, Removable, 24"
- Construction Sign
- Channelization Device
- Temporary Concrete Barrier, Type 2
- Construction Area

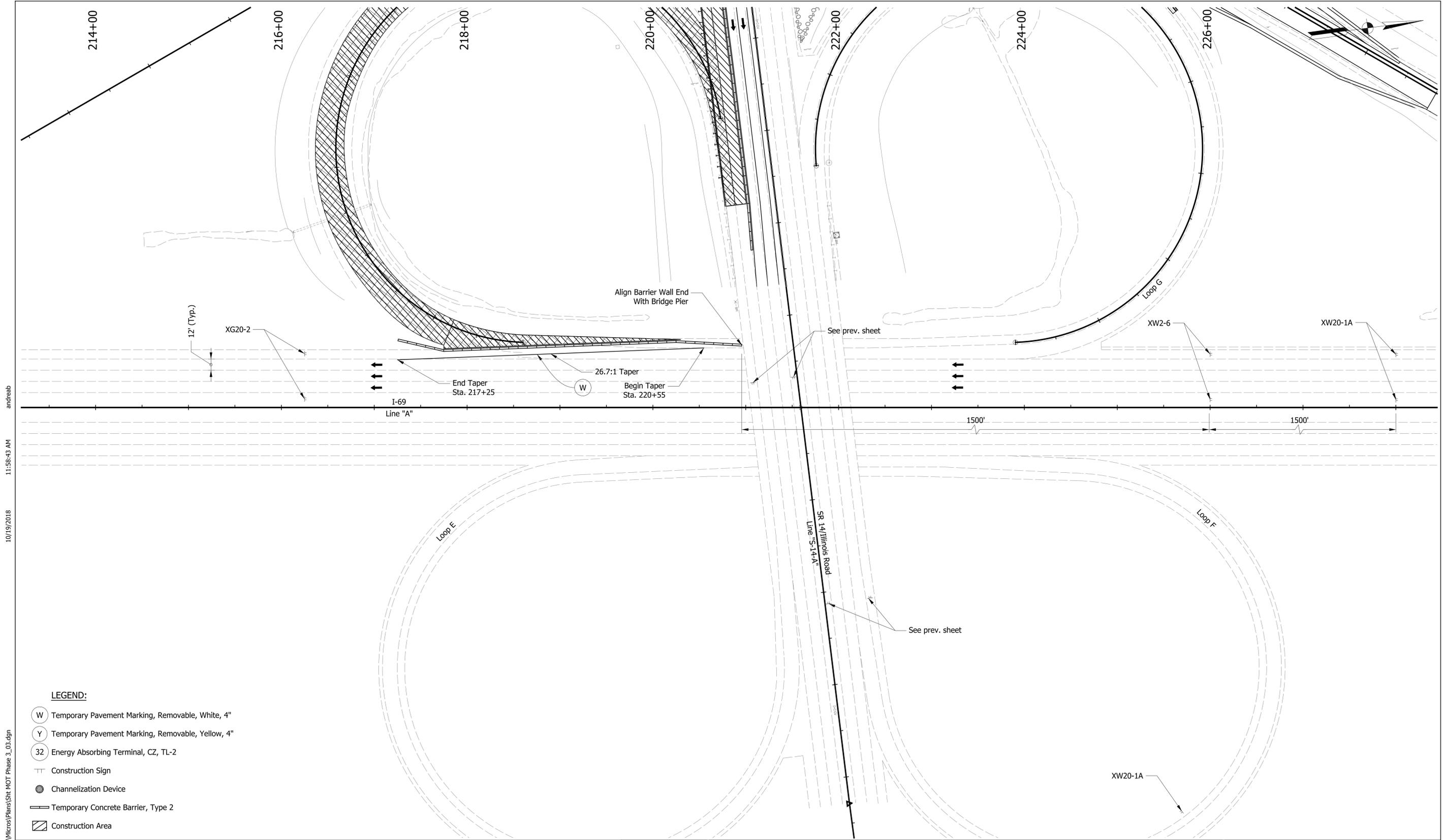
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB		DRAWN: ACB	
CHECKED: MAR		CHECKED: MAR	

INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC

PHASE 3 - SR 14

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 30'	N/A
VERTICAL SCALE	DESIGNATION NO.
N/A	1401828
SURVEY BOOK NO.	SHEETS
	12 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828



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LEGEND:

- (W) Temporary Pavement Marking, Removable, White, 4"
- (Y) Temporary Pavement Marking, Removable, Yellow, 4"
- (32) Energy Absorbing Terminal, CZ, TL-2
- ⏏ Construction Sign
- ⊙ Channelization Device
- ▬ Temporary Concrete Barrier, Type 2
- ▨ Construction Area

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB
CHECKED: MAR	CHECKED: MAR

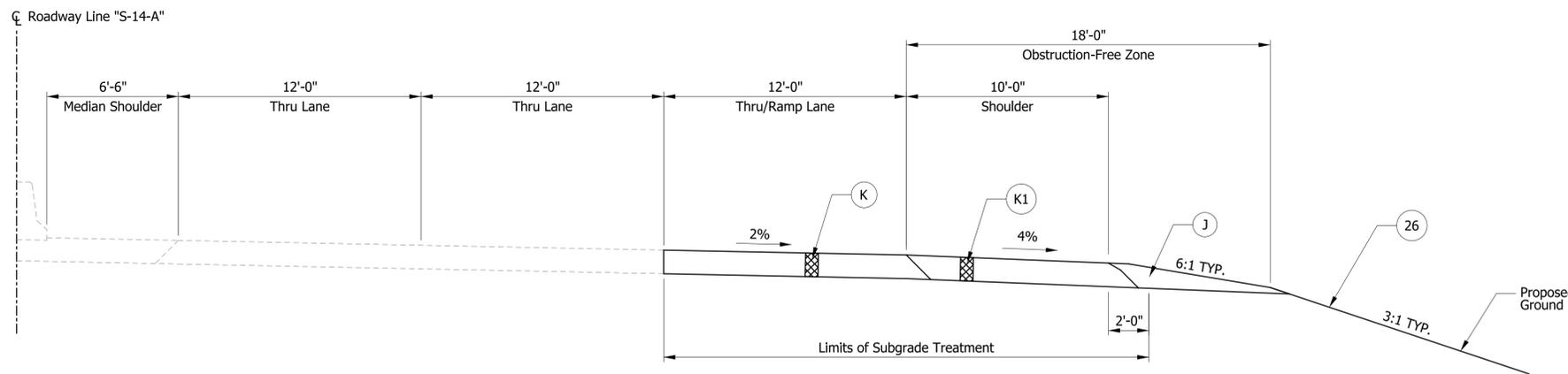
INDIANA DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PHASE 3 - I-69 & LOOP F

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE NO. N/A
VERTICAL SCALE N/A	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 13 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828

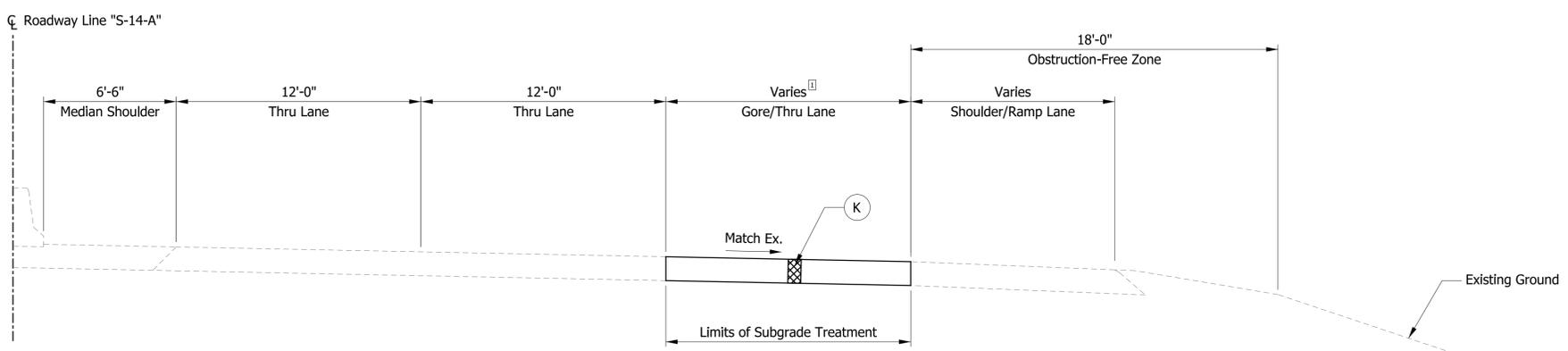
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PROPOSED TYPICAL SECTION: SR 14/ILLINOIS RD.

Sta. 40+19.13 "S-14-A" to Sta. 47+75.52 "S-14-A" EB



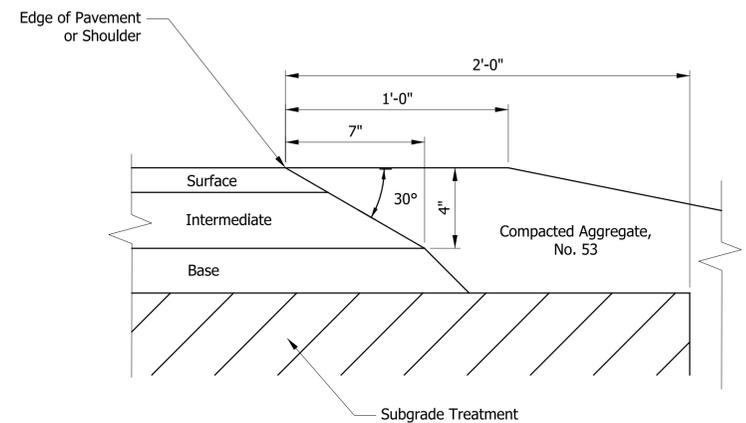
PROPOSED TYPICAL SECTION: SR 14/ILLINOIS RD.

Sta. 39+16.12 "S-14-A" to Sta. 40+19.13 "S-14-A" EB

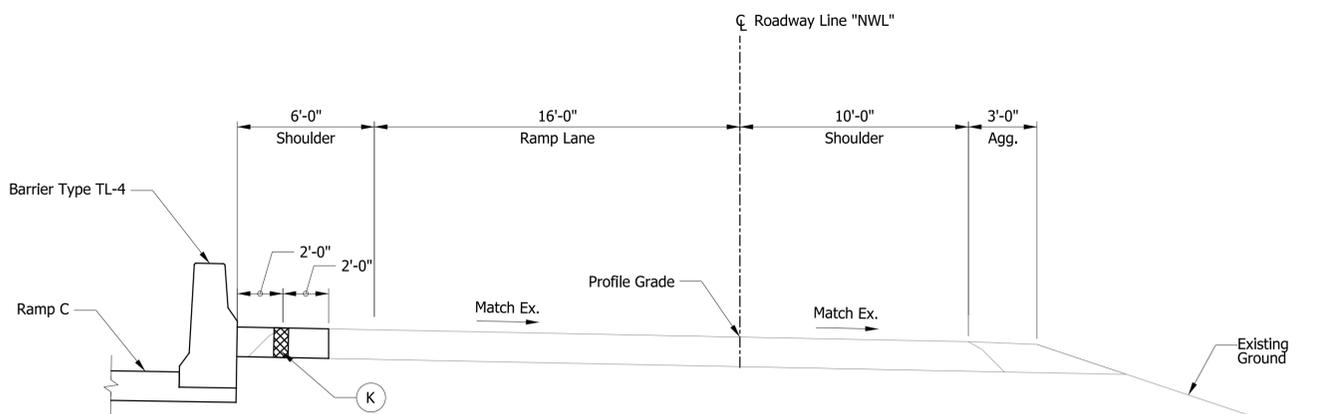
Varies from 3.09' through 26.61' from Sta. 39+16.12 to Sta. 40+19.13

- LEGEND**
- (K) Full Depth HMA Pavement
 - (K1) HMA Shoulder
 - (J) Compacted Aggregate, No. 53
 - (R) 165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
 - (26) Seeding

NOTE: SR 14/Illinois Road and Ramp G resurfacing to be completed under Des. 1600115.



SAFETY EDGE ON HMA PAVEMENT



TYPICAL SECTION: RAMP G

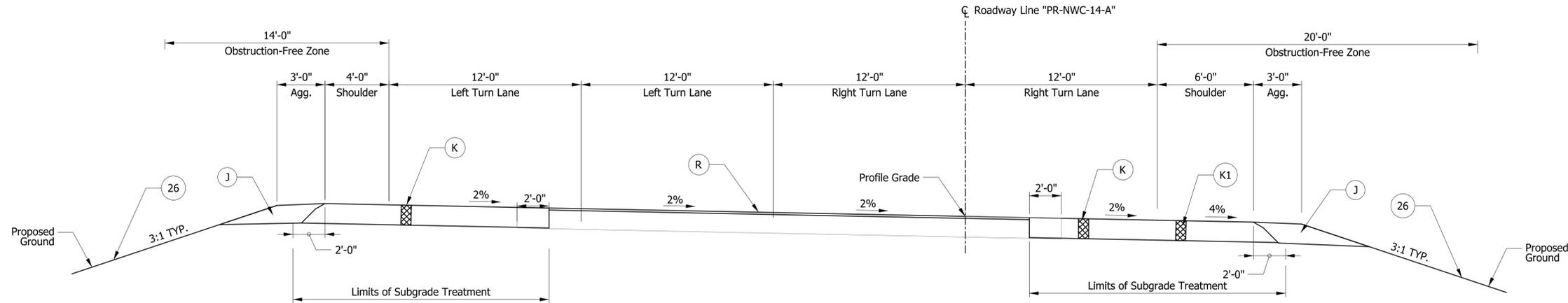
Sta. 4+27.75 "NWL" to Sta. 5+30.00 "NWL"

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA DEPARTMENT OF TRANSPORTATION

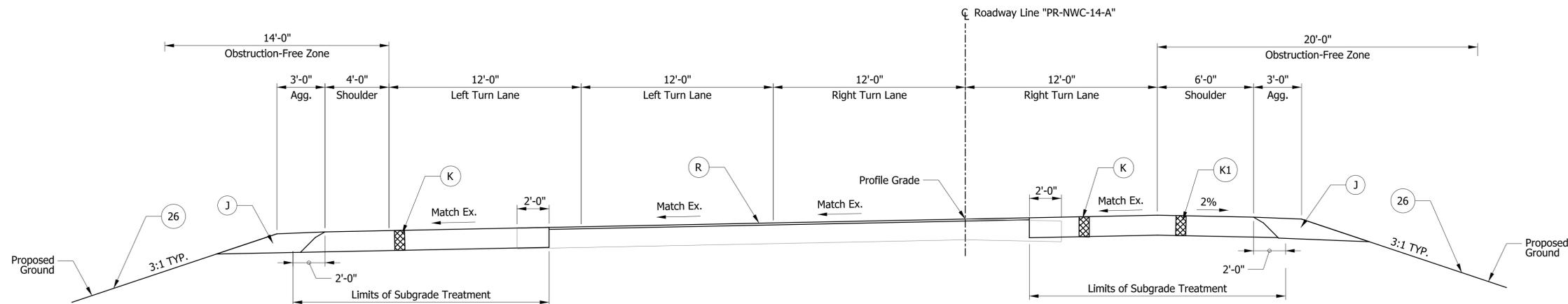
TYPICAL CROSS SECTION I-69 AT SR 14/ILLINOIS RD

HORIZONTAL SCALE	BRIDGE FILE NO.
1/4" = 1'-0"	NA
VERTICAL SCALE	DESIGNATION NO.
	1401828
SURVEY BOOK NO.	SHEETS
	14 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828



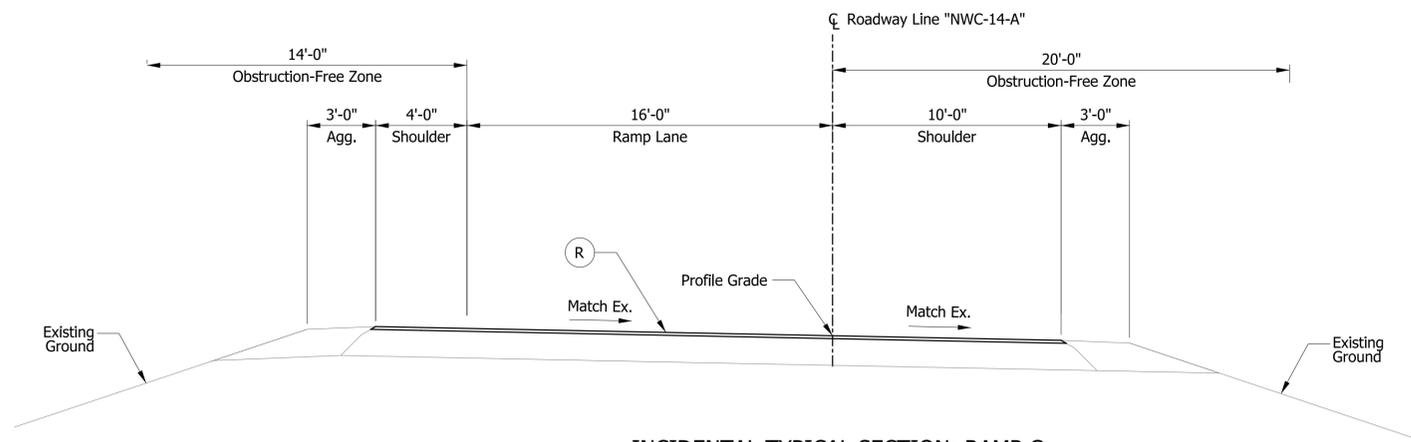
PROPOSED TYPICAL SECTION: RAMP C

Sta. 7+13.00 "PR-NWC-14-A" to Sta. 11+25.00 "PR-NWC-14-A"



PROPOSED SUPERELEVATED TYPICAL SECTION: RAMP C

Sta. 11+25.00 "PR-NWC-14-A" to Sta. 15+01.83 "PR-NWC-14-A"



INCIDENTAL TYPICAL SECTION: RAMP C

Sta. 1+27.40 "NWC-14-A" to Sta. 7+13.00 "NWC-14-A"

LEGEND

- (K) Full Depth HMA Pavement
- (R) 165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
- (K1) HMA Shoulder
- (26) Seeding
- (J) Compacted Aggregate, No. 53

NOTE: SR 14/Illinois Road resurfacing to be completed under Des. 1600115.

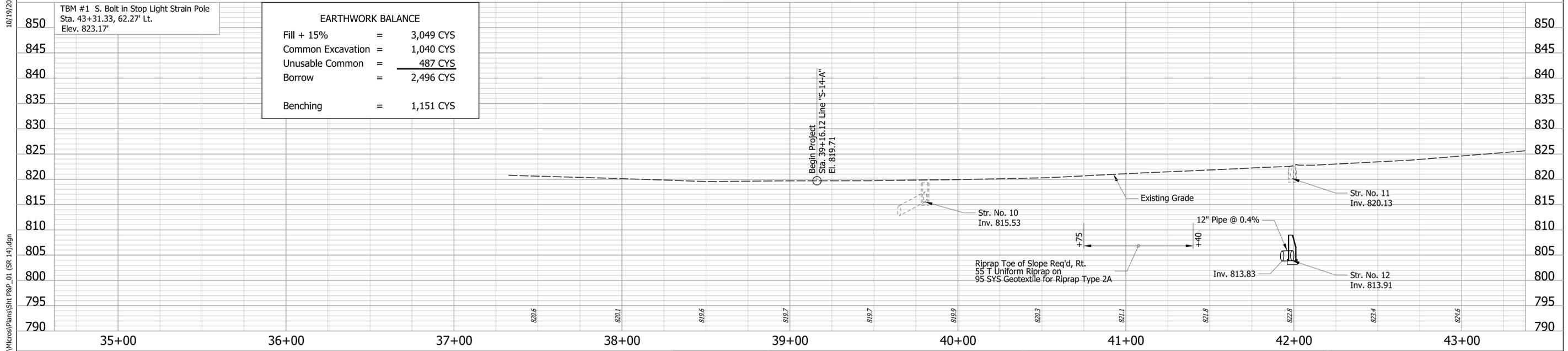
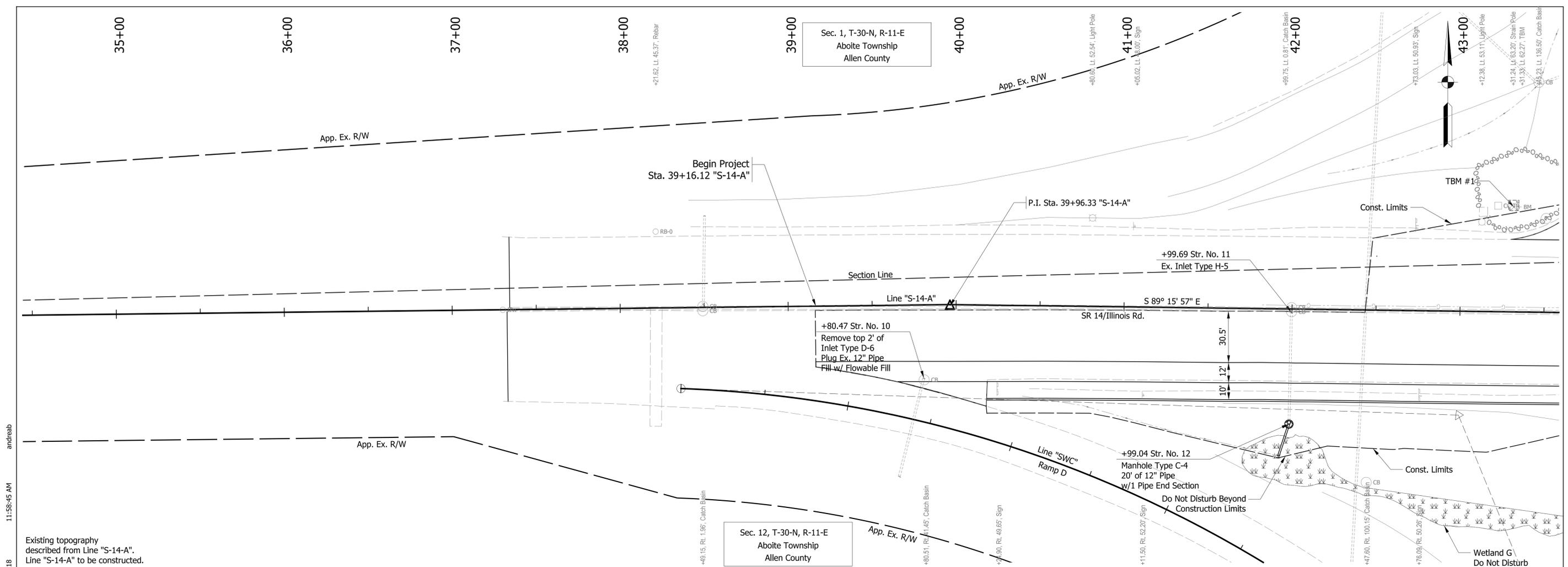
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RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA DEPARTMENT OF TRANSPORTATION

TYPICAL CROSS SECTION I-69 AT SR 14/ILLINOIS RD

HORIZONTAL SCALE	BRIDGE FILE NO.
1/4" = 1'-0"	NA
VERTICAL SCALE	DESIGNATION NO.
	1401828
SURVEY BOOK NO.	SHEETS
	15 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828



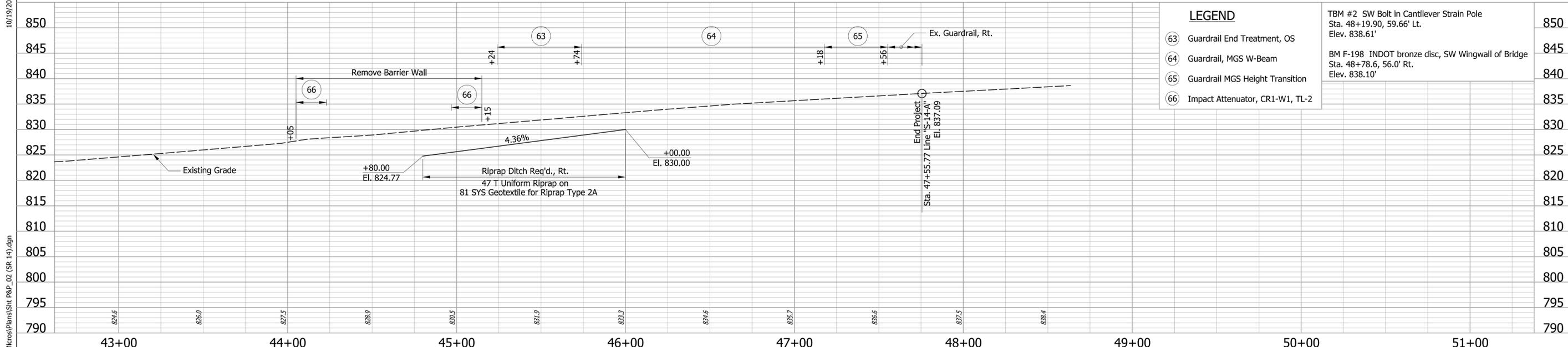
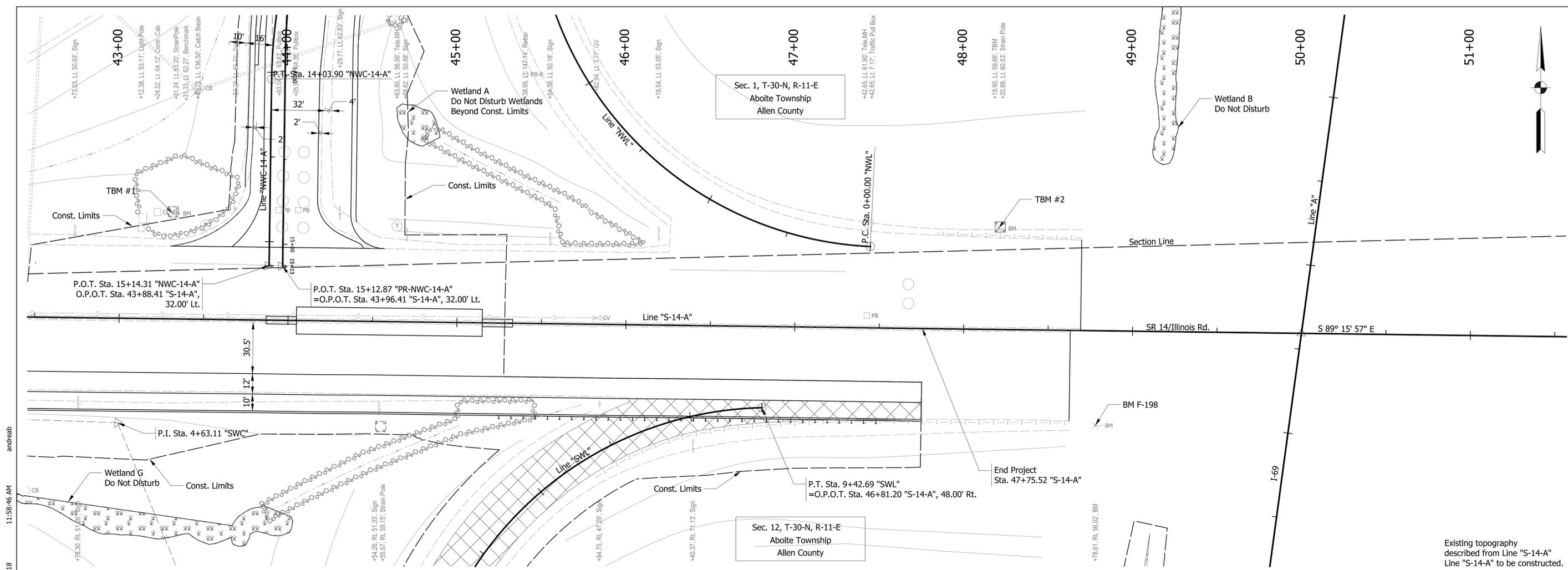
EARTHWORK BALANCE		
Fill + 15%	=	3,049 CYS
Common Excavation	=	1,040 CYS
Unusable Common	=	487 CYS
Borrow	=	2,496 CYS
Benching	=	1,151 CYS

Existing topography described from Line "S-14-A".
Line "S-14-A" to be constructed.

TBM #1 S. Bolt in Stop Light Strain Pole
Sta. 43+31.33, 62.27' Lt.
Elev. 823.17'

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RECOMMENDED FOR APPROVAL	DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 30'	BRIDGE FILE NO. N/A
	DESIGNED: ALB DRAWN: ACB		VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1401828
CHECKED: MAR	CHECKED: MAR	PLAN AND PROFILE LINE "S-14-A"	SURVEY BOOK NO.	SHEETS 16 of 48
			CONTRACT NO. R-38565	PROJECT NO. 1401828

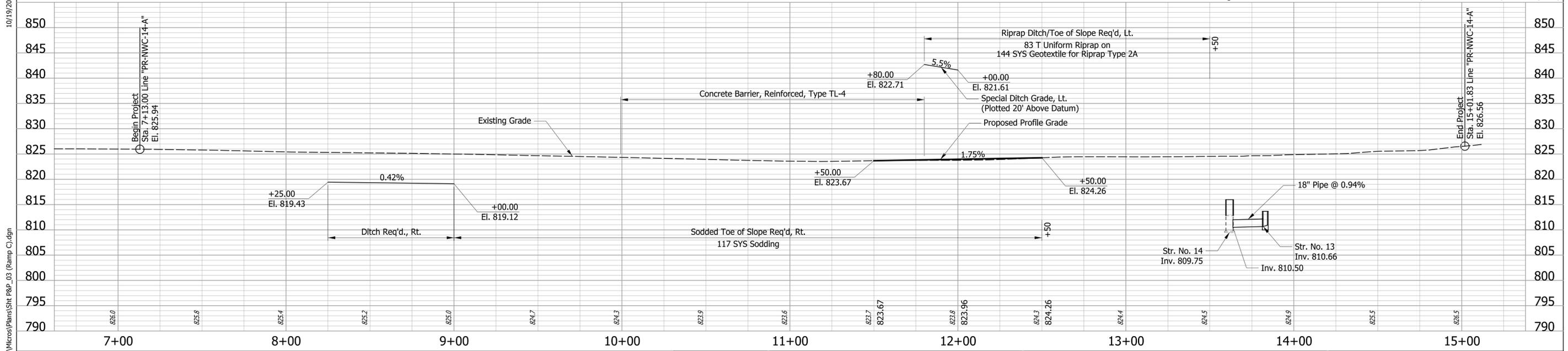
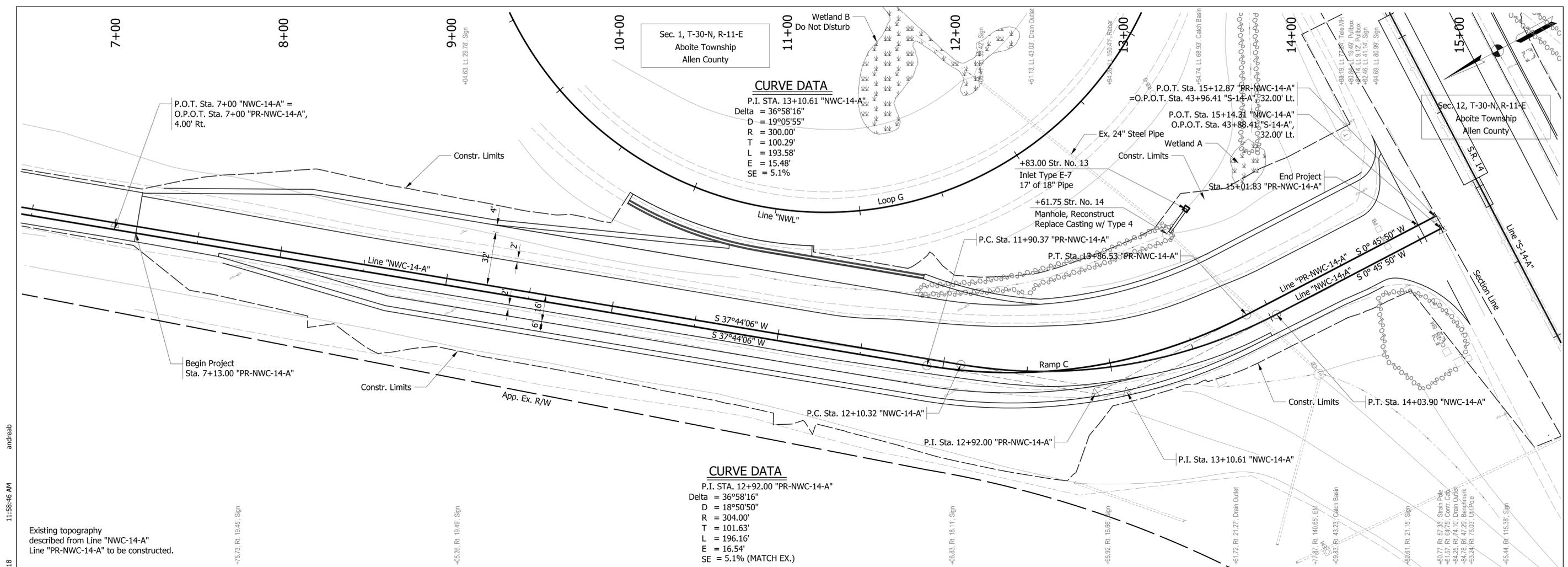


LEGEND	
63	Guardrail End Treatment, OS
64	Guardrail, MGS W-Beam
65	Guardrail MGS Height Transition
66	Impact Attenuator, CR1-W1, TL-2

TBM #2 SW Bolt in Cantilever Strain Pole Sta. 48+19.90, 59.66' Lt. Elev. 838.61'	850
BM F-198 INDOT bronze disc, SW Wingwall of Bridge Sta. 48+78.6, 56.0' Rt. Elev. 838.10'	845

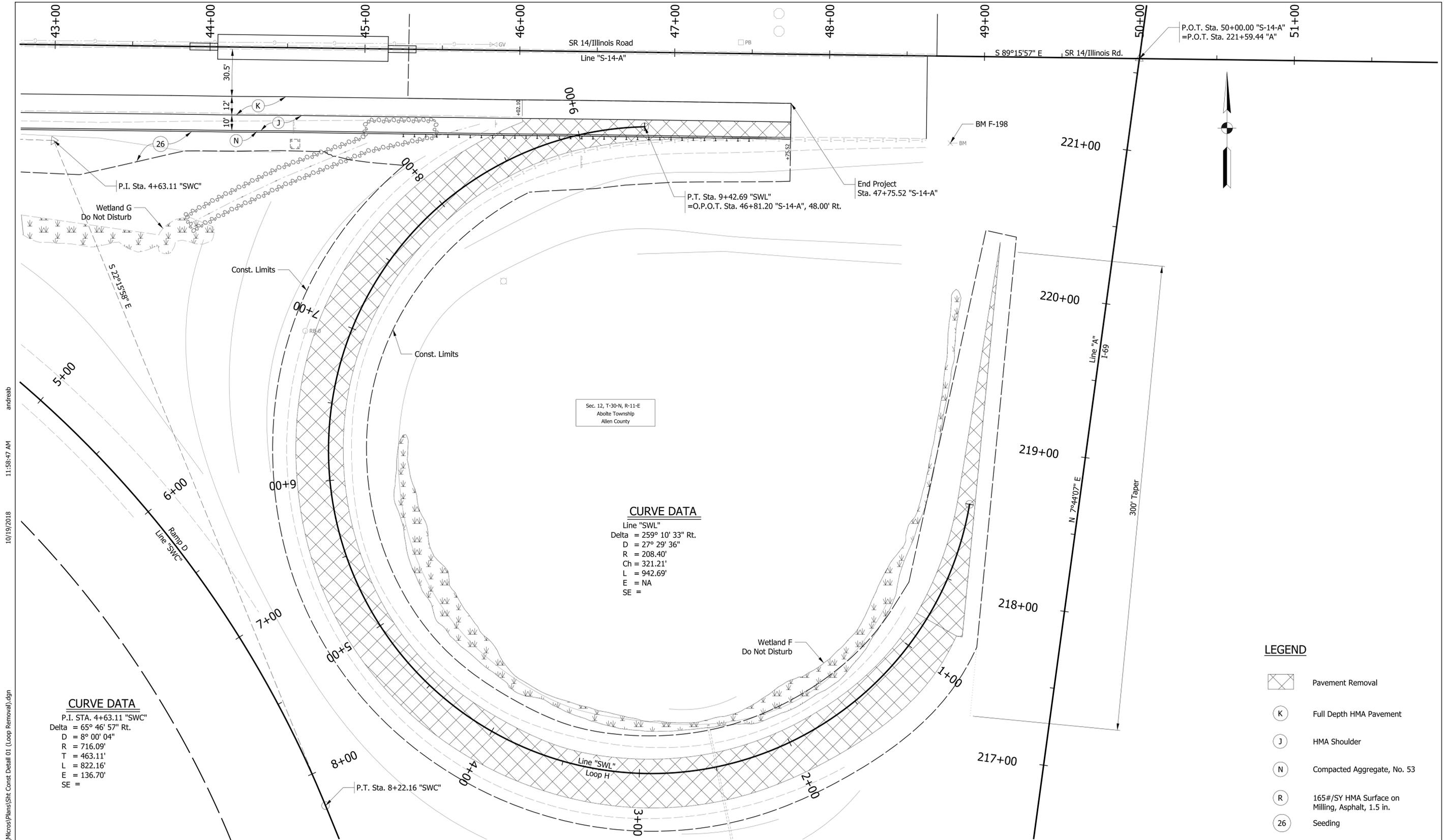
RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION PLAN AND PROFILE LINE "S-14-A"	HORIZONTAL SCALE 1" = 30'	BRIDGE FILE NO. N/A
DESIGNED: ALB	DRAWN: ACB		VERTICAL SCALE 1" = 10'	DESIGNATION NO. 1401828
CHECKED: MAR	CHECKED: MAR	SURVEY BOOK NO.	SHEETS 17 of 48	
		CONTRACT NO. R-38565	PROJECT NO. 1401828	

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RECOMMENDED FOR APPROVAL		DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE 1" = 30'		BRIDGE FILE NO. N/A	
DESIGNED: ALB		DRAWN: ACB				VERTICAL SCALE 1" = 10'		DESIGNATION NO. 1401828	
CHECKED: MAR		CHECKED: MAR		PLAN & PROFILE LINE "PR-NWC-14-A"		SURVEY BOOK NO.		SHEETS 18 of 48	
						CONTRACT NO. R-38565		PROJECT NO. 1401828	

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LEGEND

	Pavement Removal
	Full Depth HMA Pavement
	HMA Shoulder
	Compacted Aggregate, No. 53
	165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
	Seeding

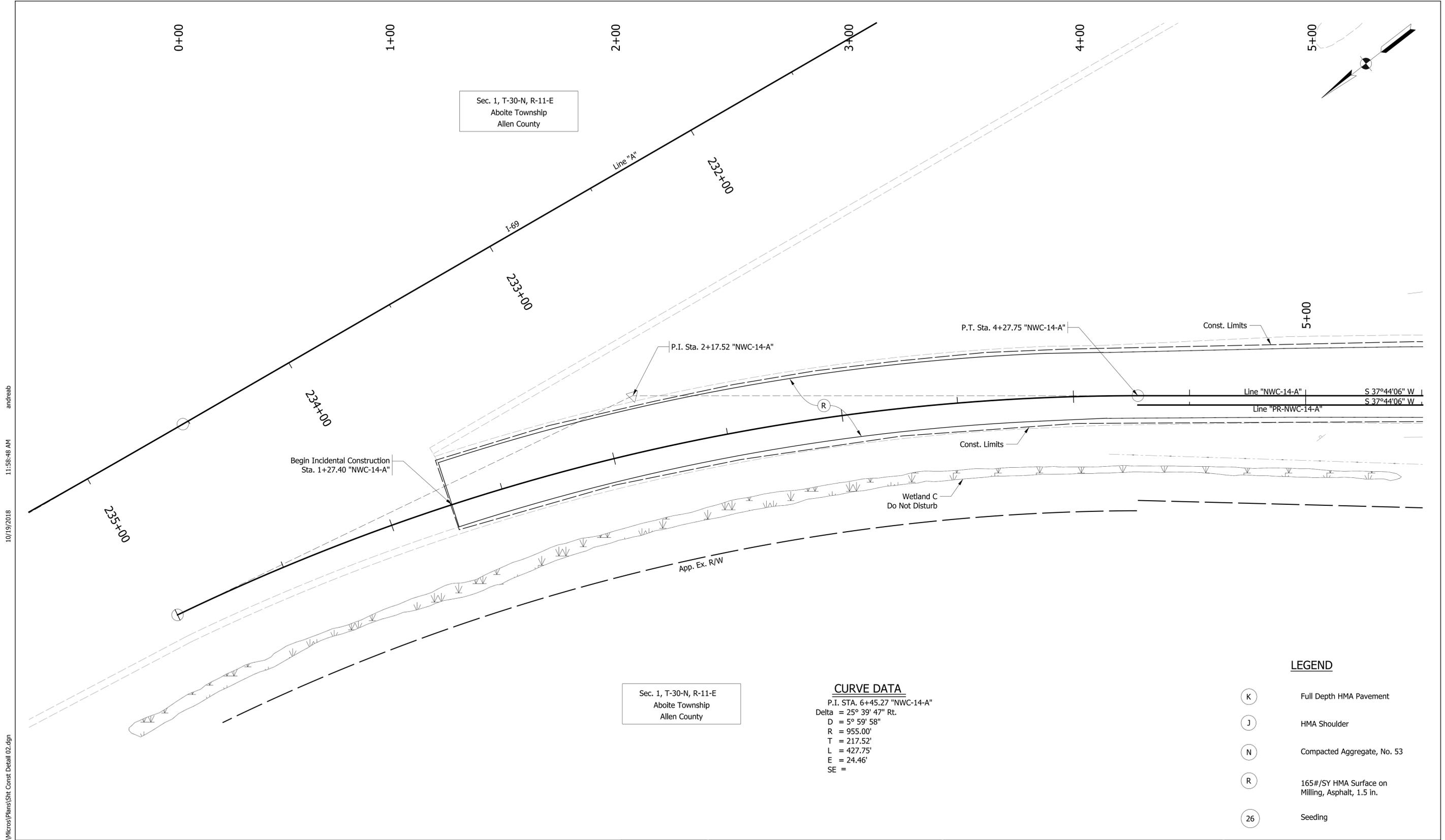
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RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE
DESIGNED: ALB	DRAWN: ACB			
CHECKED: MAR	CHECKED: MAR			

INDIANA DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LOOP H REMOVAL

HORIZONTAL SCALE 1" = 30'	BRIDGE FILE NO. N/A
VERTICAL SCALE NA	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 19 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828



Sec. 1, T-30-N, R-11-E
Aboite Township
Allen County

Sec. 1, T-30-N, R-11-E
Aboite Township
Allen County

CURVE DATA
 P.I. STA. 6+45.27 "NWC-14-A"
 Delta = 25° 39' 47" Rt.
 D = 5° 59' 58"
 R = 955.00'
 T = 217.52'
 L = 427.75'
 E = 24.46'
 SE =

LEGEND

- (K) Full Depth HMA Pavement
- (J) HMA Shoulder
- (N) Compacted Aggregate, No. 53
- (R) 165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
- (26) Seeding

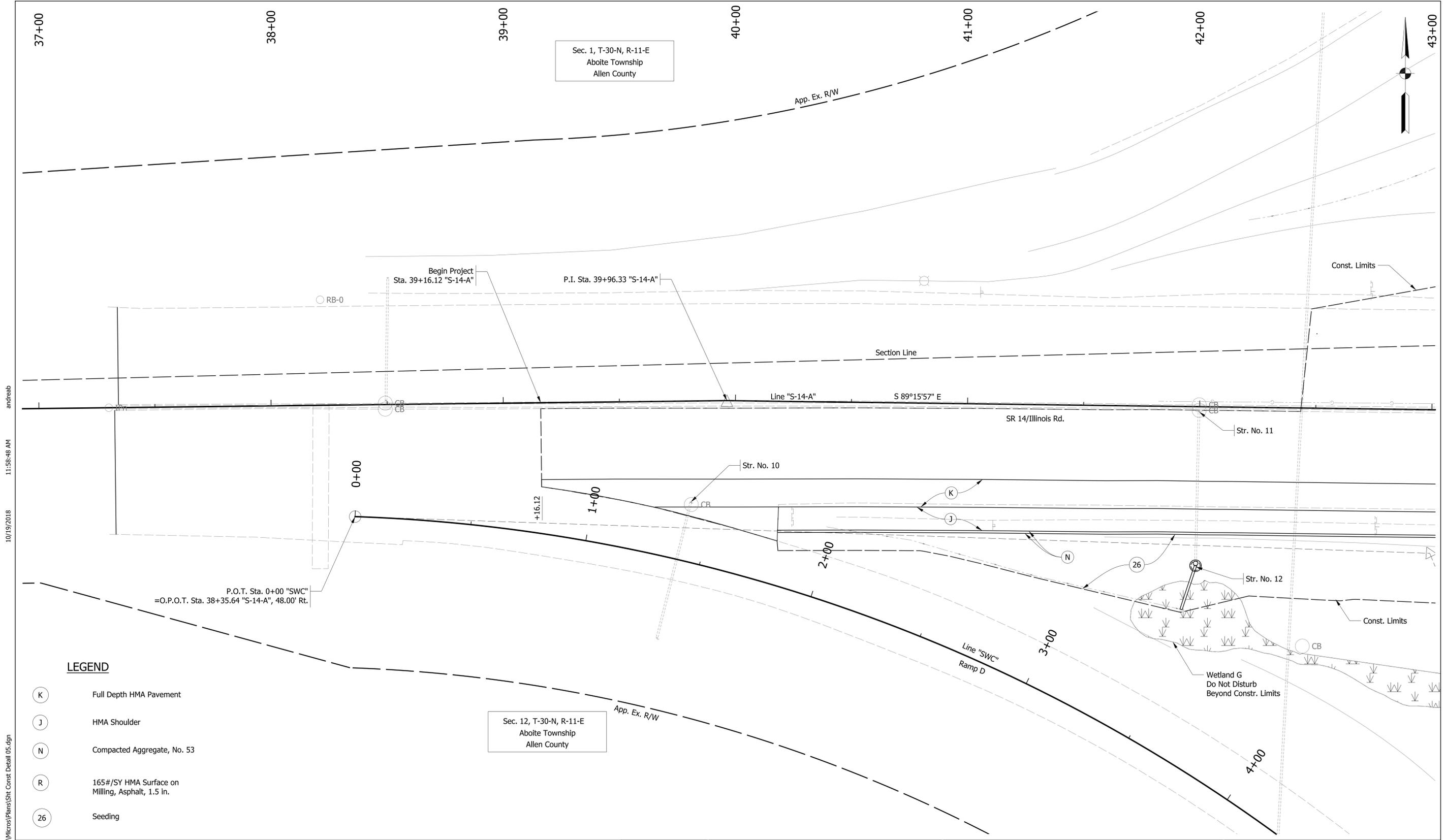
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RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAILS
LINE "NWC-14-A"**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE NO. N/A
VERTICAL SCALE NA	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 20 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828



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LEGEND

- (K) Full Depth HMA Pavement
- (J) HMA Shoulder
- (N) Compacted Aggregate, No. 53
- (R) 165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
- (26) Seeding

Sec. 12, T-30-N, R-11-E
 Aboite Township
 Allen County

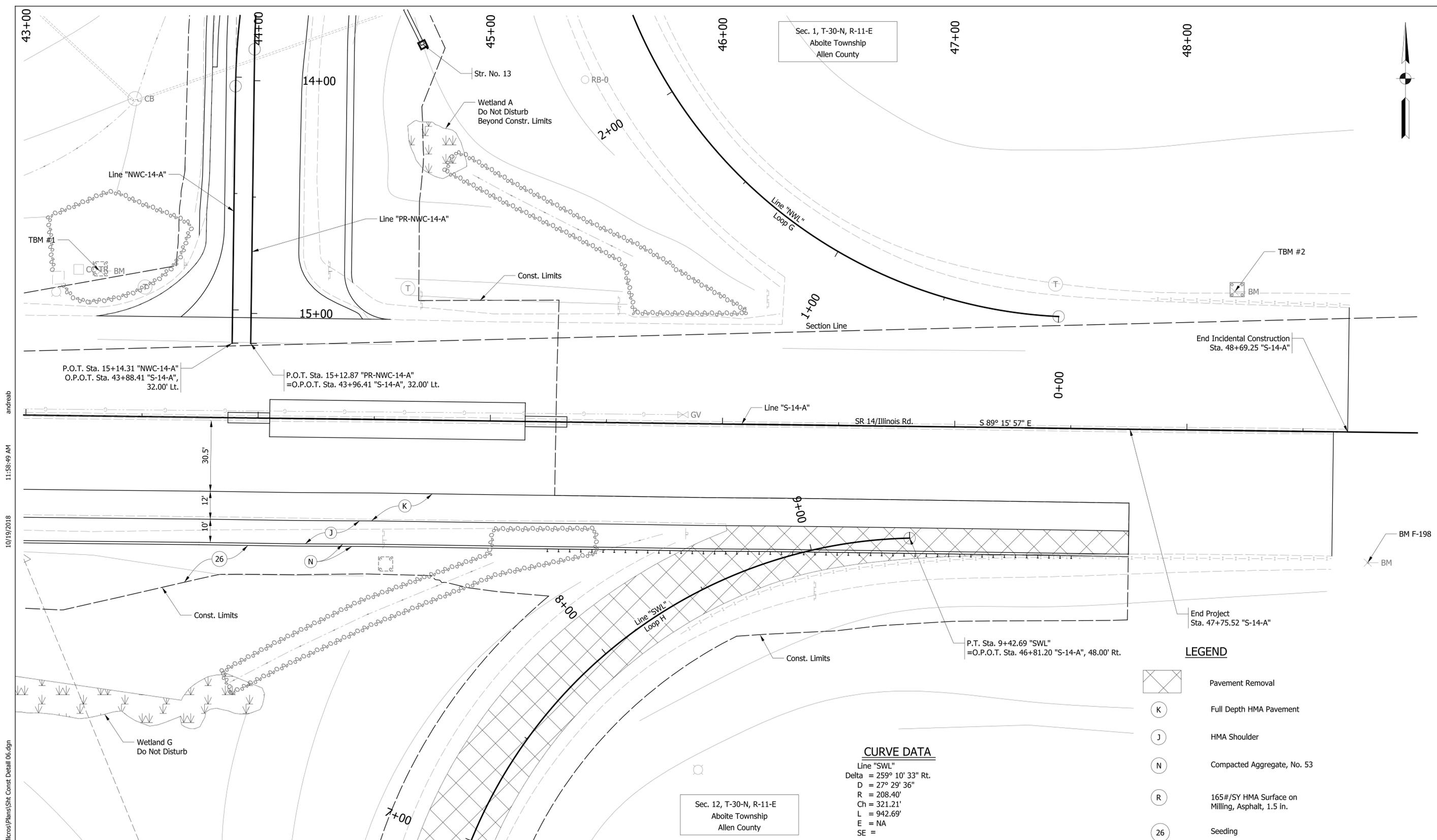
Sec. 1, T-30-N, R-11-E
 Aboite Township
 Allen County

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB
CHECKED: MAR	CHECKED: MAR

INDIANA DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "S-14-A"

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION NO.
N/A	1401828
SURVEY BOOK NO.	SHEETS
	21 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828



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Sec. 1, T-30-N, R-11-E
 Aboite Township
 Allen County

Sec. 12, T-30-N, R-11-E
 Aboite Township
 Allen County

CURVE DATA
 Line "SWL"
 Delta = 259° 10' 33" Rt.
 D = 27° 29' 36"
 R = 208.40'
 Ch = 321.21'
 L = 942.69'
 E = NA
 SE =

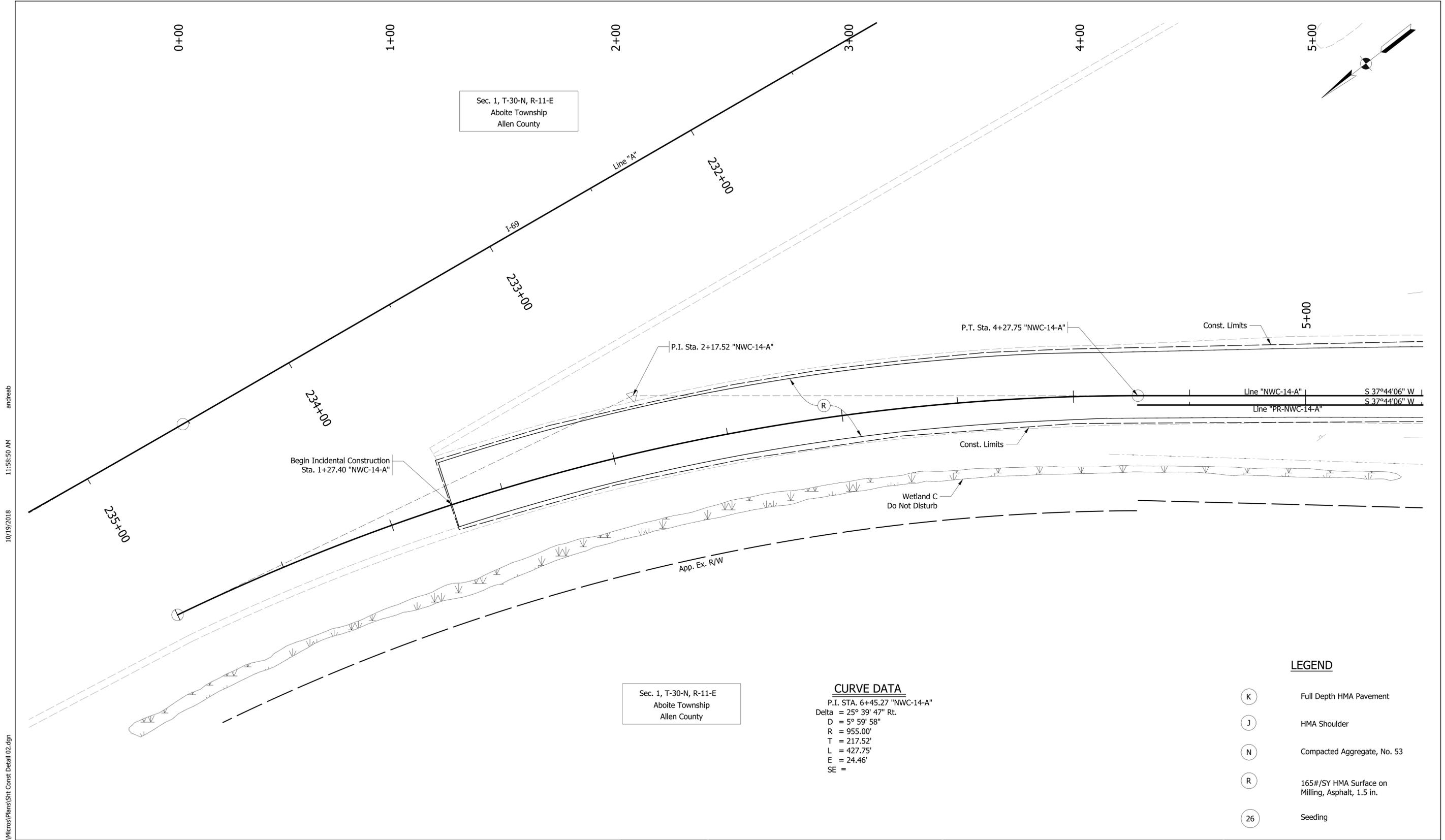
- LEGEND**
-  Pavement Removal
 -  Full Depth HMA Pavement
 -  HMA Shoulder
 -  Compacted Aggregate, No. 53
 -  165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
 -  Seeding

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS
LINE "S-14-A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE NO. N/A
VERTICAL SCALE N/A	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 22 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828



Sec. 1, T-30-N, R-11-E
Aboite Township
Allen County

Sec. 1, T-30-N, R-11-E
Aboite Township
Allen County

CURVE DATA
 P.I. STA. 6+45.27 "NWC-14-A"
 Delta = 25° 39' 47" Rt.
 D = 5° 59' 58"
 R = 955.00'
 T = 217.52'
 L = 427.75'
 E = 24.46'
 SE =

LEGEND

- (K) Full Depth HMA Pavement
- (J) HMA Shoulder
- (N) Compacted Aggregate, No. 53
- (R) 165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
- (26) Seeding

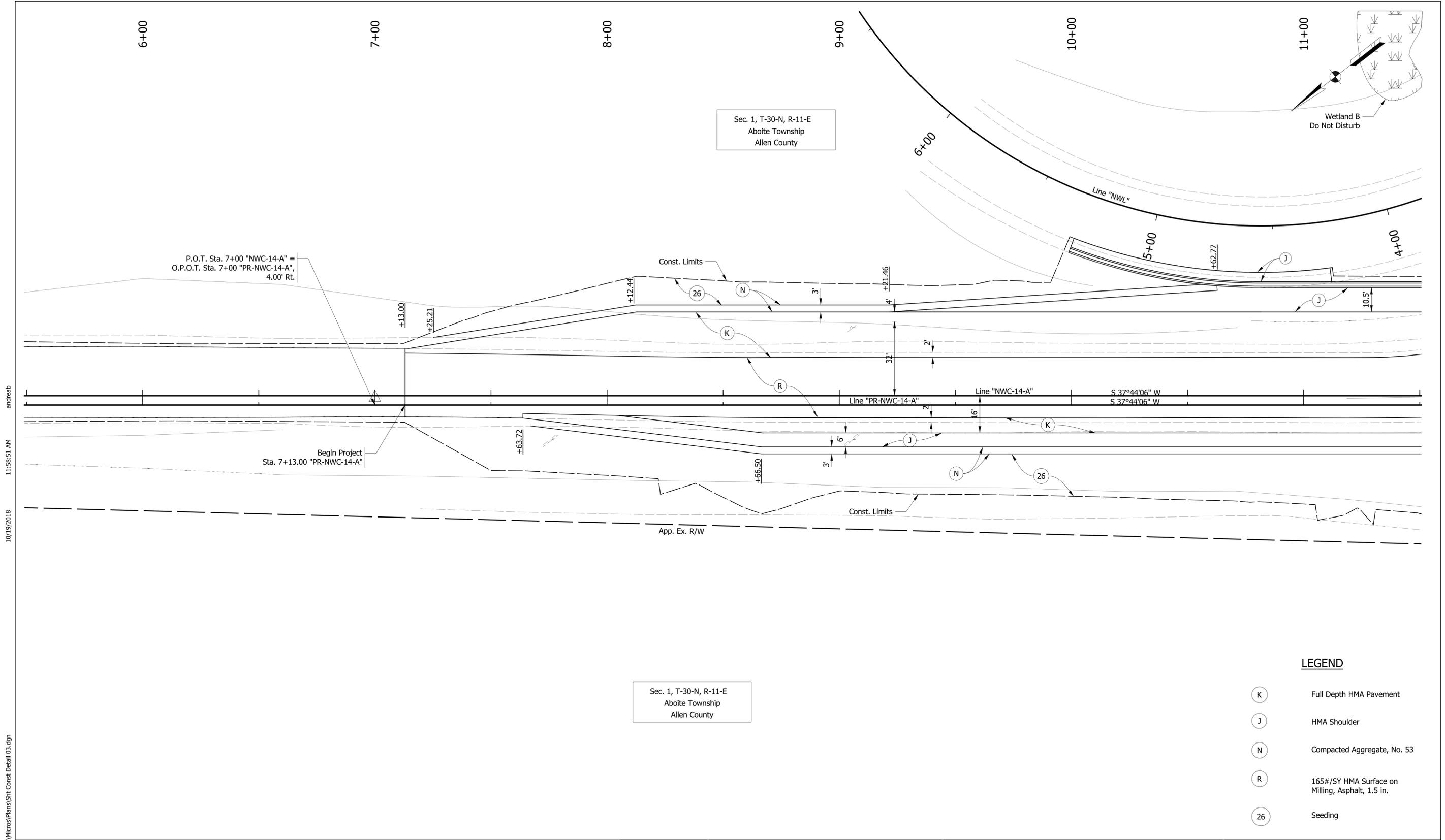
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RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAILS
LINE "NWC-14-A"**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE NO. N/A
VERTICAL SCALE NA	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 24 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828



Sec. 1, T-30-N, R-11-E
Aboite Township
Allen County

Sec. 1, T-30-N, R-11-E
Aboite Township
Allen County

LEGEND

- (K) Full Depth HMA Pavement
- (J) HMA Shoulder
- (N) Compacted Aggregate, No. 53
- (R) 165#/SY HMA Surface on Milling, Asphalt, 1.5 in.
- (26) Seeding

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RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

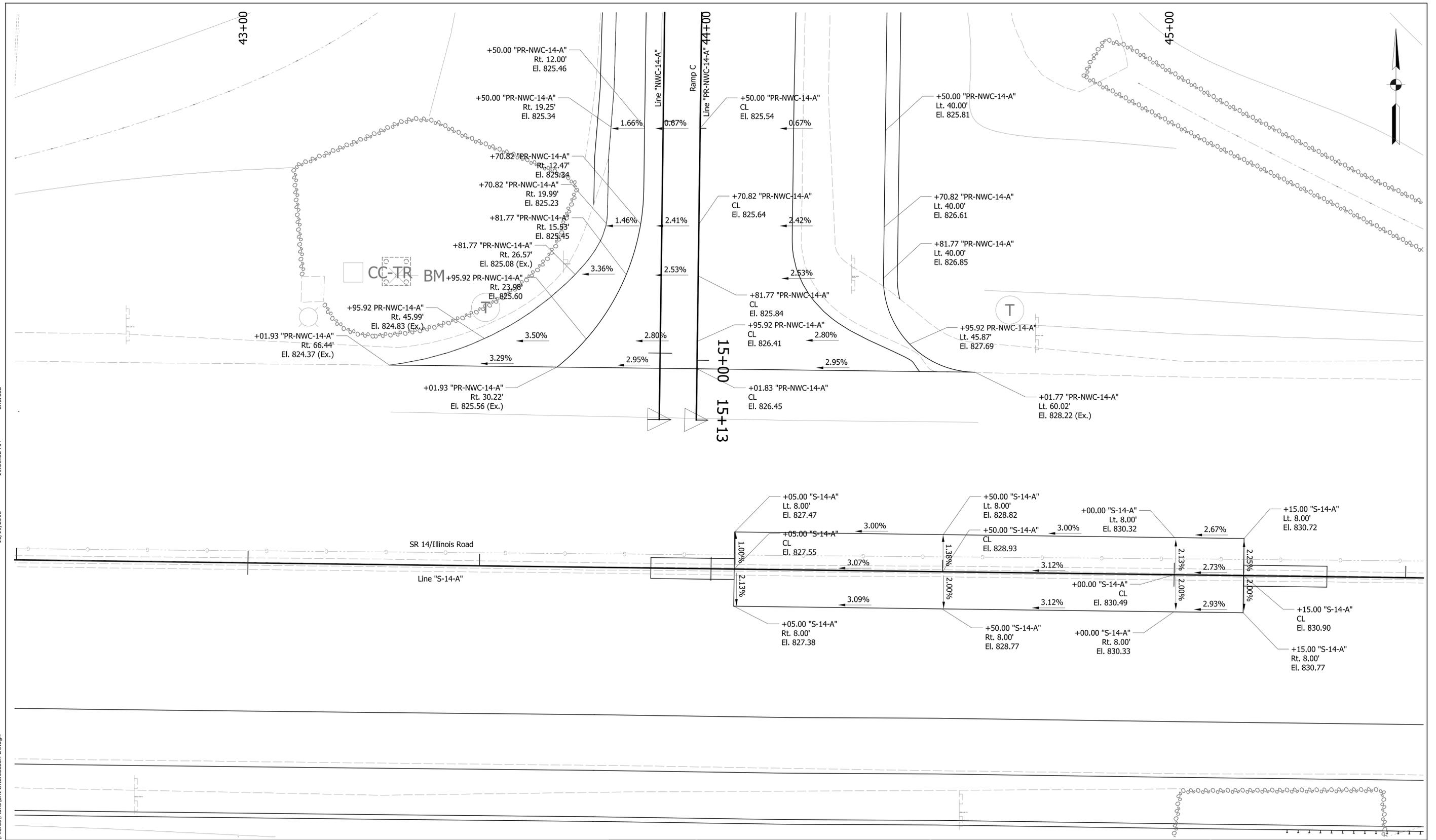
**INDIANA
DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION DETAILS
LINE "NWC-14-A"**

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE NO. N/A
VERTICAL SCALE NA	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 25 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828

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DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA DEPARTMENT OF TRANSPORTATION

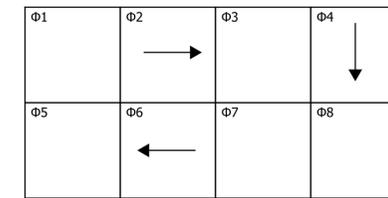
INTERSECTION DETAIL

HORIZONTAL SCALE 1" = 10'	BRIDGE FILE NO. N/A
VERTICAL SCALE	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 27 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828

LEGEND

- Ex. Signal Strain Pole, Steel 36 FT, on Existing Foundation
- Signal Strain Pole, Steel 36 FT, on Proposed Foundation
- Pavement Message Markings, Thermoplastic, Lane Indication Arrow
- TS2 Controller and Type P-1 Cabinet
Min. 8 Phase on Type P-1 Foundation
- Existing Controller Cabinet and Foundation
- Existing Traffic Signal Handhole
- Existing Signal Strain Pole and Foundation
- Disconnect Hanger
- Detector Housing
- Signal Handhole Type I
- Loop Detector
- 2" HDPE Schedule 80 Conduit
- Panel Sign
- Proposed Traffic Signal Head, 3 Section, 12 IN.
- TRAFFIC SIGNAL HEAD, 3 FACE, 12" :
RED, LT. AMBER ARROW, LT. GREEN ARROW
- TRAFFIC SIGNAL HEAD, 3 FACE, 12" :
RED, RT. AMBER ARROW, RT. GREEN ARROW

PHASE DIAGRAM



LOOP TAGGING TABLE	
LANE	TAG - NUMBER
EA	EA2 - 1
EB	EB2 - 1
EC	EC2 - 1
WA	WA6 - 1
WB	WB6 - 1
SRA	SRA4 - 1, 2, 3, 4
SRB	SRB4 - 1, 2, 3, 4
SLA	SLB4 - 1, 2, 3, 4
SLB	SLB4 - 1, 2, 3, 4



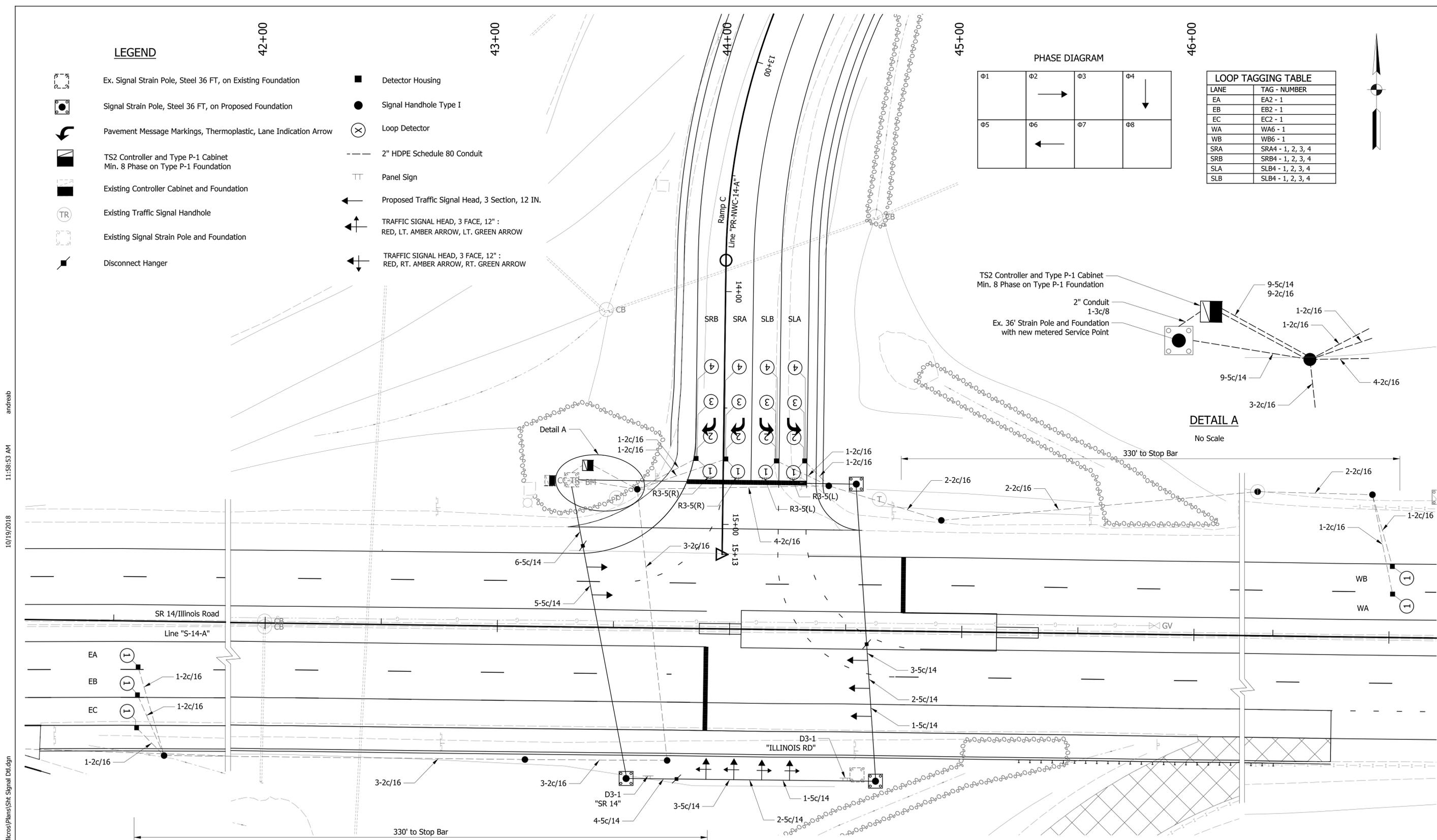
42+00

43+00

44+00

45+00

46+00



TS2 Controller and Type P-1 Cabinet
Min. 8 Phase on Type P-1 Foundation

2" Conduit
1-3c/8
Ex. 36' Strain Pole and Foundation
with new metered Service Point

DETAIL A

No Scale

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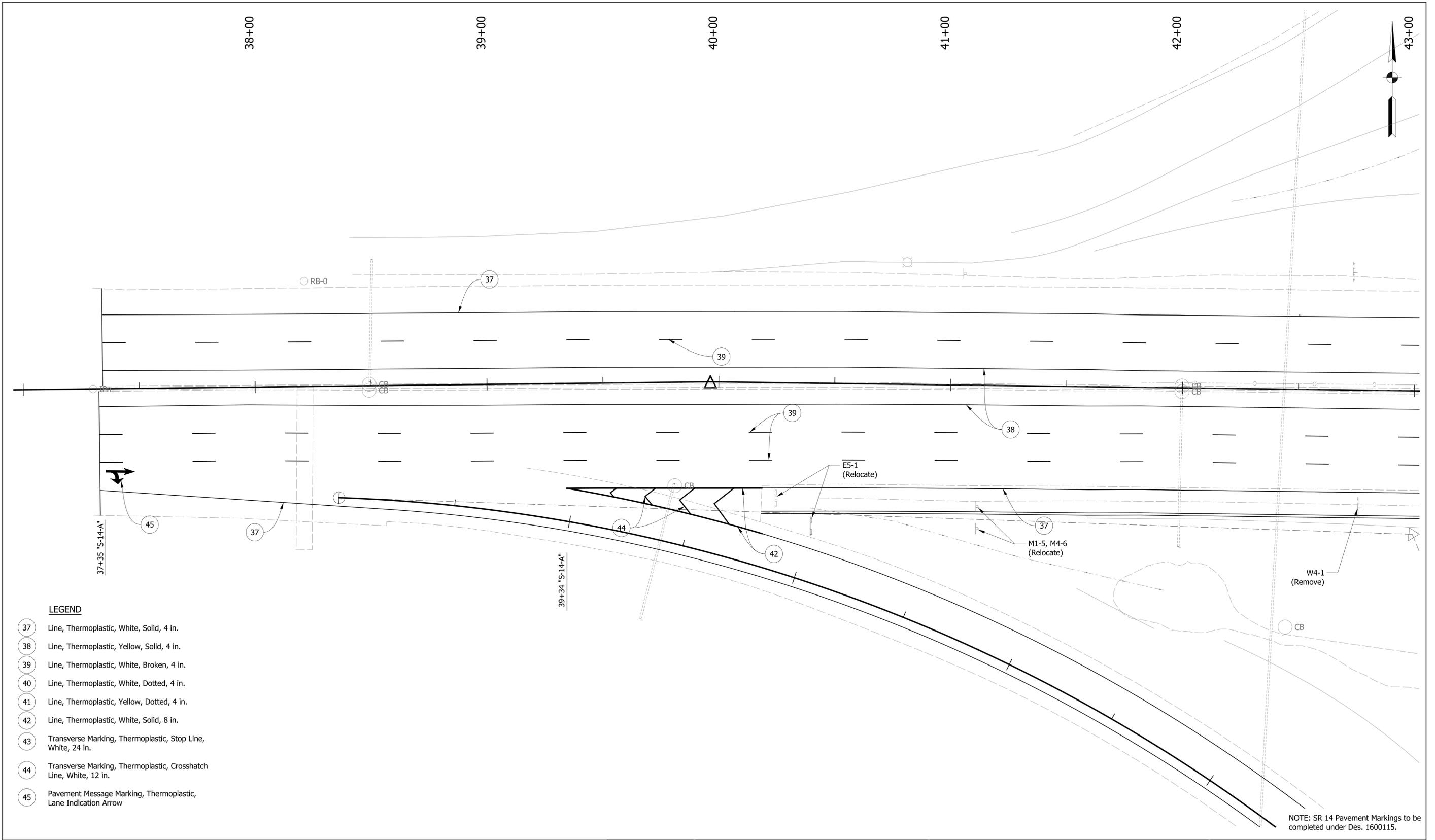
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB	
CHECKED: MAR	CHECKED: MAR	

**INDIANA
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODERNIZATION
I-69 AT SR 14/ILLINOIS RD**

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION NO.
	1401828
SURVEY BOOK NO.	SHEETS
	28 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828

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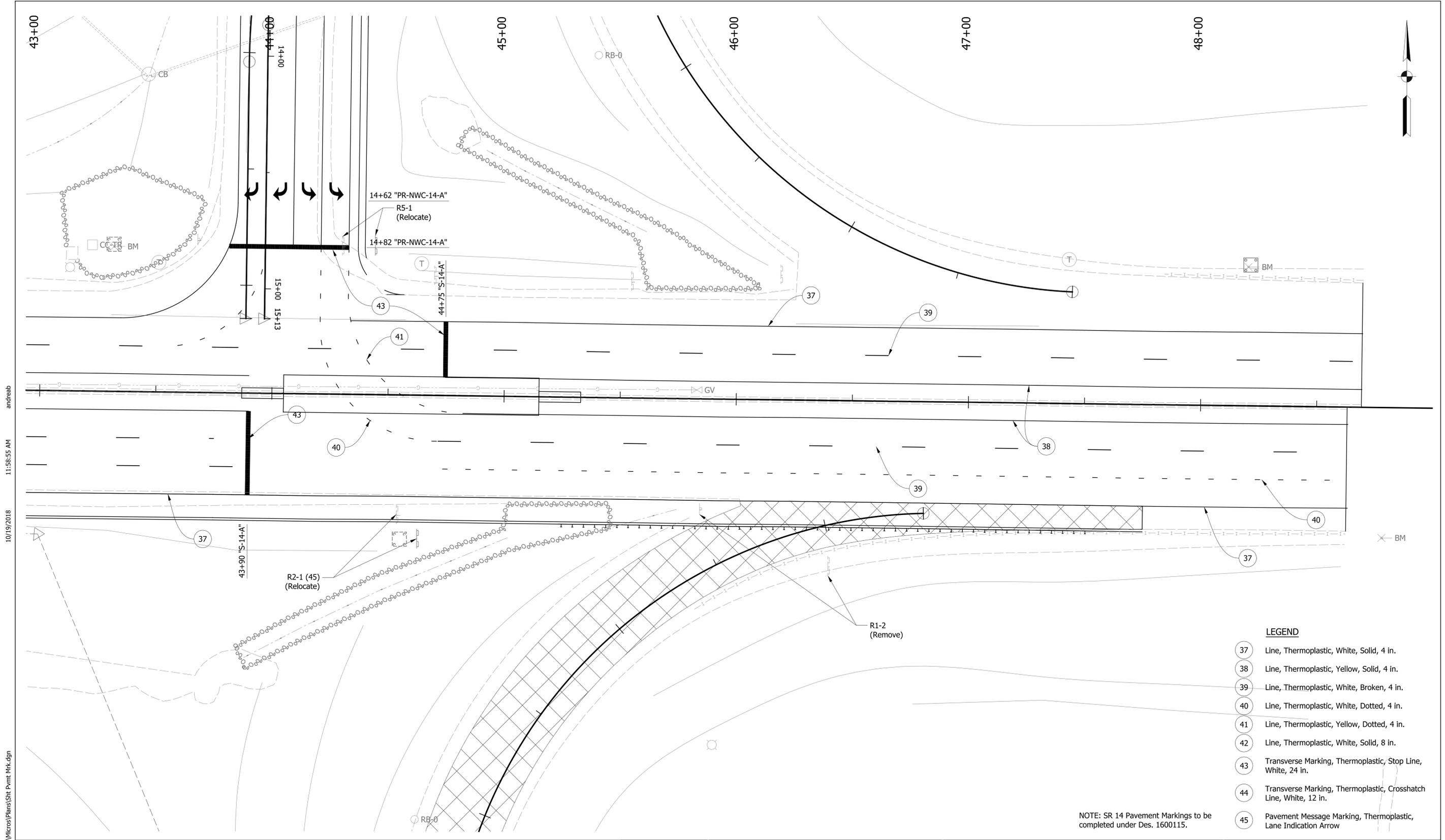


LEGEND

- 37 Line, Thermoplastic, White, Solid, 4 in.
- 38 Line, Thermoplastic, Yellow, Solid, 4 in.
- 39 Line, Thermoplastic, White, Broken, 4 in.
- 40 Line, Thermoplastic, White, Dotted, 4 in.
- 41 Line, Thermoplastic, Yellow, Dotted, 4 in.
- 42 Line, Thermoplastic, White, Solid, 8 in.
- 43 Transverse Marking, Thermoplastic, Stop Line, White, 24 in.
- 44 Transverse Marking, Thermoplastic, Crosshatch Line, White, 12 in.
- 45 Pavement Message Marking, Thermoplastic, Lane Indication Arrow

NOTE: SR 14 Pavement Markings to be completed under Des. 1600115.

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 20' VERTICAL SCALE NA	BRIDGE FILE NO. N/A DESIGNATION NO. 1401828
DESIGNED: ALB CHECKED: MAR	DRAWN: ACB CHECKED: MAR	PAVEMENT MARKING & SIGNAGE PLANS LINE "S-14-A"	
		SURVEY BOOK NO. R-38565	SHEETS 29 of 48 PROJECT NO. 1401828



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LEGEND

37	Line, Thermoplastic, White, Solid, 4 in.
38	Line, Thermoplastic, Yellow, Solid, 4 in.
39	Line, Thermoplastic, White, Broken, 4 in.
40	Line, Thermoplastic, White, Dotted, 4 in.
41	Line, Thermoplastic, Yellow, Dotted, 4 in.
42	Line, Thermoplastic, White, Solid, 8 in.
43	Transverse Marking, Thermoplastic, Stop Line, White, 24 in.
44	Transverse Marking, Thermoplastic, Crosshatch Line, White, 12 in.
45	Pavement Message Marking, Thermoplastic, Lane Indication Arrow

NOTE: SR 14 Pavement Markings to be completed under Des. 1600115.

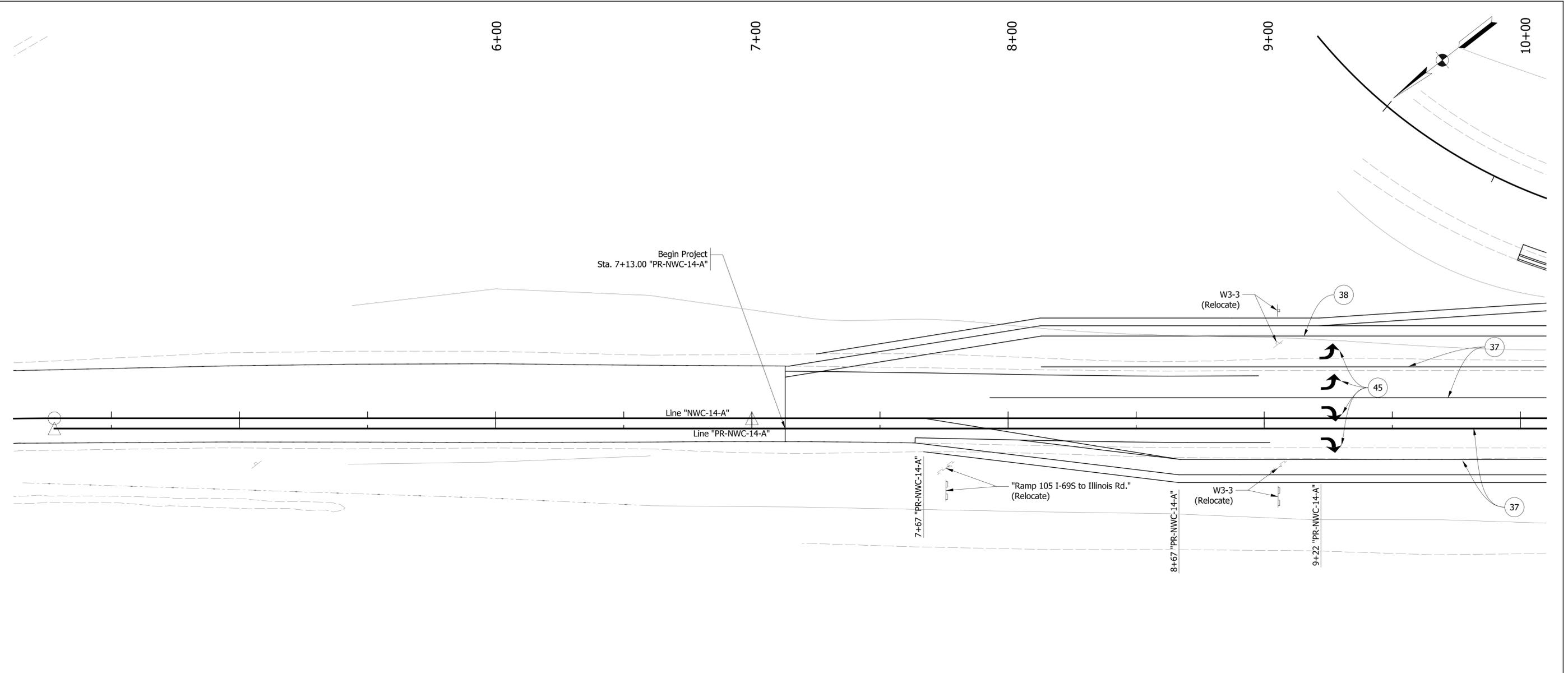
**INDIANA
 DEPARTMENT OF TRANSPORTATION**
PAVEMENT MARKING & SIGNAGE PLANS
LINE "S-14-A"

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION NO.
NA	1401828
SURVEY BOOK NO.	SHEETS
	30 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828

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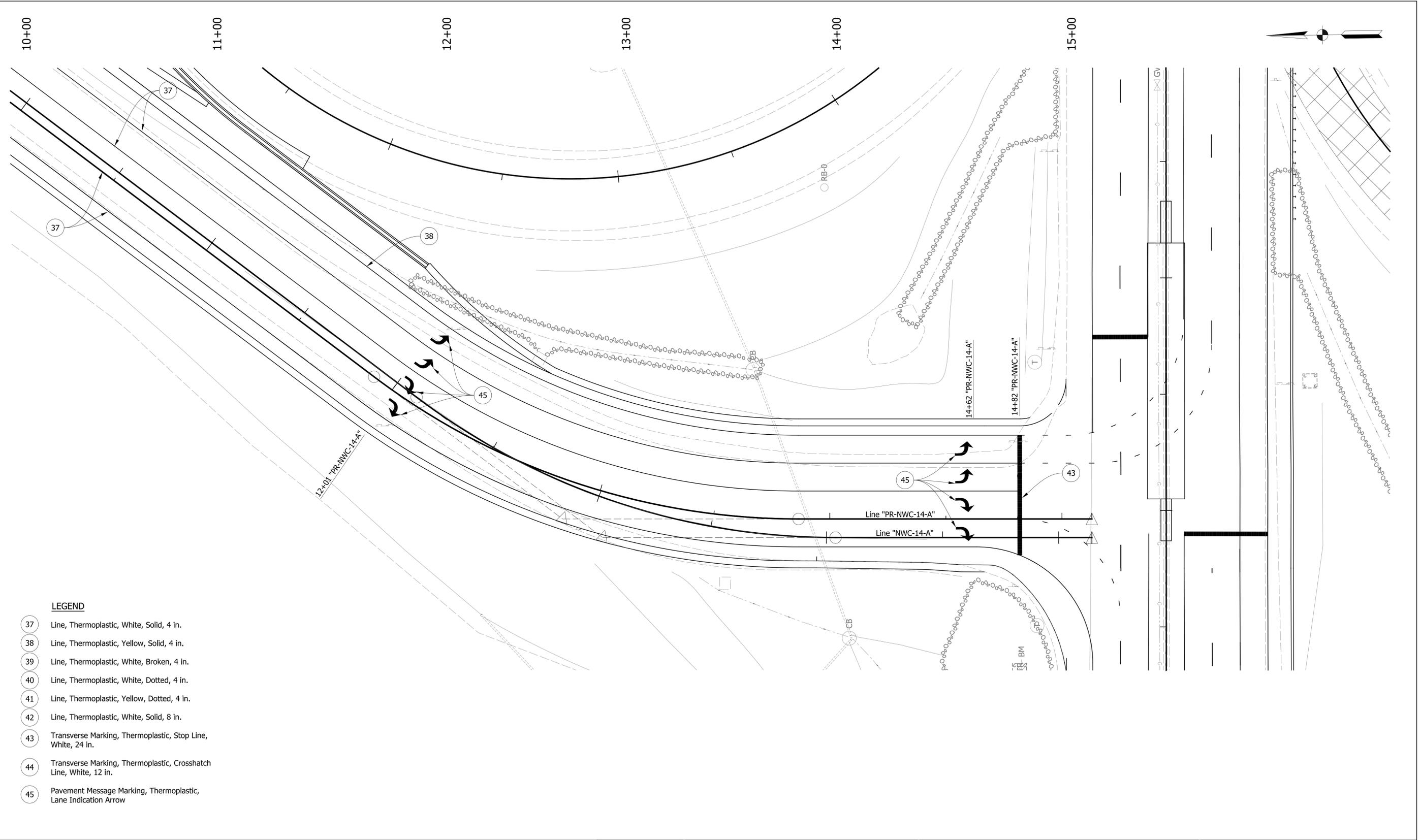
LEGEND

- 37 Line, Thermoplastic, White, Solid, 4 in.
- 38 Line, Thermoplastic, Yellow, Solid, 4 in.
- 39 Line, Thermoplastic, White, Broken, 4 in.
- 40 Line, Thermoplastic, White, Dotted, 4 in.
- 41 Line, Thermoplastic, Yellow, Dotted, 4 in.
- 42 Line, Thermoplastic, White, Solid, 8 in.
- 43 Transverse Marking, Thermoplastic, Stop Line, White, 24 in.
- 44 Transverse Marking, Thermoplastic, Crosshatch Line, White, 12 in.
- 45 Pavement Message Marking, Thermoplastic, Lane Indication Arrow

	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 20' VERTICAL SCALE NA	BRIDGE FILE NO. N/A DESIGNATION NO. 1401828
DESIGNED: ALB	DRAWN: ACB	PAVEMENT MARKING & SIGNAGE PLANS LINE "PR-NWC-14-A"	SURVEY BOOK NO. SHEETS 31 of 48	
CHECKED: MAR	CHECKED: MAR		CONTRACT NO. PROJECT NO. R-38565 1401828	

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- LEGEND**
- 37 Line, Thermoplastic, White, Solid, 4 in.
 - 38 Line, Thermoplastic, Yellow, Solid, 4 in.
 - 39 Line, Thermoplastic, White, Broken, 4 in.
 - 40 Line, Thermoplastic, White, Dotted, 4 in.
 - 41 Line, Thermoplastic, Yellow, Dotted, 4 in.
 - 42 Line, Thermoplastic, White, Solid, 8 in.
 - 43 Transverse Marking, Thermoplastic, Stop Line, White, 24 in.
 - 44 Transverse Marking, Thermoplastic, Crosshatch Line, White, 12 in.
 - 45 Pavement Message Marking, Thermoplastic, Lane Indication Arrow

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

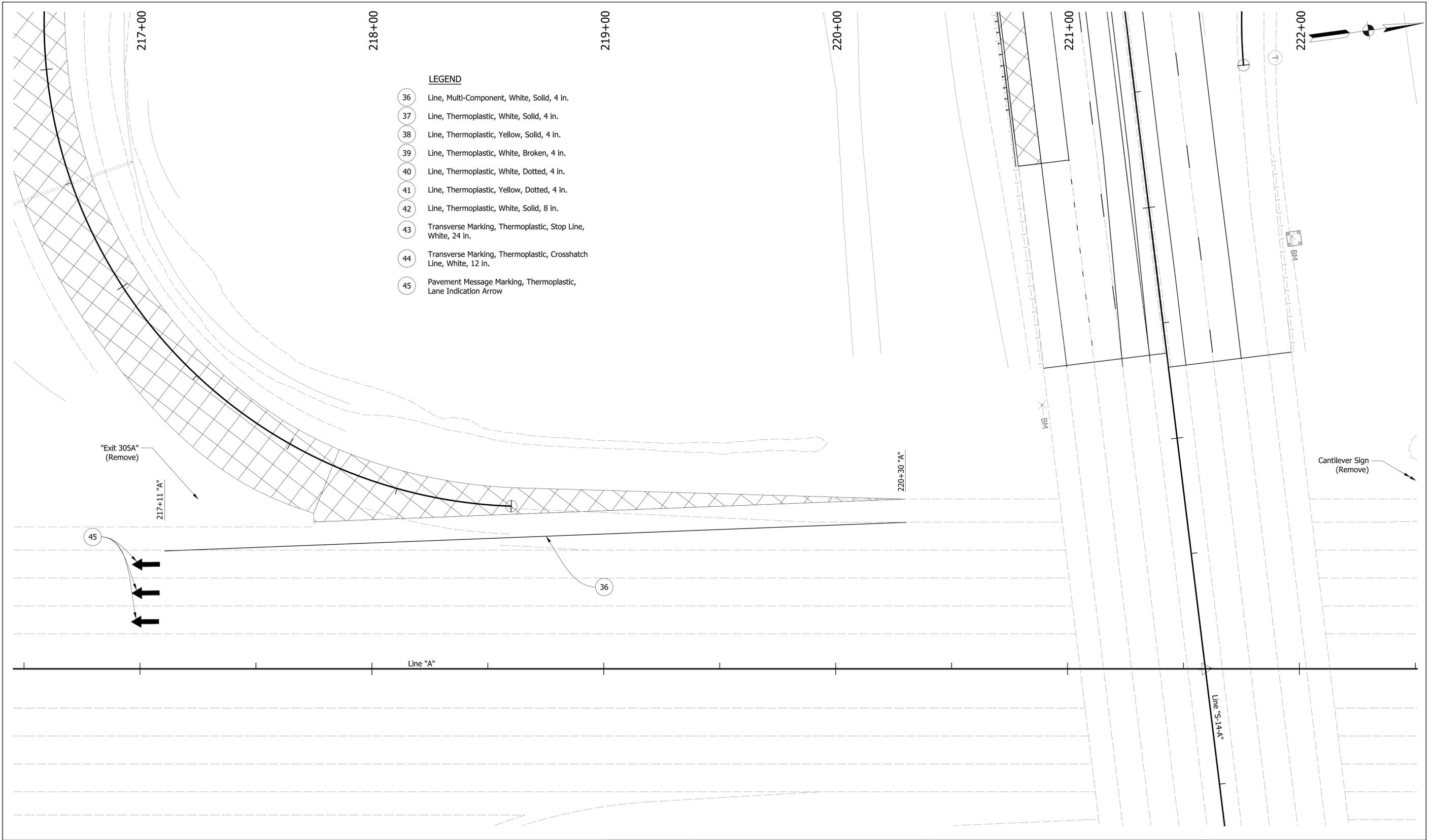
INDIANA DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING & SIGNAGE PLANS

LINE "PR-NWC-14-A"

HORIZONTAL SCALE	BRIDGE FILE NO.
1" = 20'	N/A
VERTICAL SCALE	DESIGNATION NO.
NA	1401828
SURVEY BOOK NO.	SHEETS
	32 of 48
CONTRACT NO.	PROJECT NO.
R-38565	1401828

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- LEGEND**
- 36 Line, Multi-Component, White, Solid, 4 in.
 - 37 Line, Thermoplastic, White, Solid, 4 in.
 - 38 Line, Thermoplastic, Yellow, Solid, 4 in.
 - 39 Line, Thermoplastic, White, Broken, 4 in.
 - 40 Line, Thermoplastic, White, Dotted, 4 in.
 - 41 Line, Thermoplastic, Yellow, Dotted, 4 in.
 - 42 Line, Thermoplastic, White, Solid, 8 in.
 - 43 Transverse Marking, Thermoplastic, Stop Line, White, 24 in.
 - 44 Transverse Marking, Thermoplastic, Crosshatch Line, White, 12 in.
 - 45 Pavement Message Marking, Thermoplastic, Lane Indication Arrow

RECOMMENDED FOR APPROVAL _____		DESIGN ENGINEER _____	DATE _____
DESIGNED: ALB	DRAWN: ACB		
CHECKED: MAR	CHECKED: MAR		

INDIANA DEPARTMENT OF TRANSPORTATION
PAVEMENT MARKING & SIGNAGE PLANS
LINE "A"

HORIZONTAL SCALE 1" = 20'	BRIDGE FILE NO. N/A
VERTICAL SCALE N/A	DESIGNATION NO. 1401828
SURVEY BOOK NO.	SHEETS 33 of 48
CONTRACT NO. R-38565	PROJECT NO. 1401828

APPENDIX C: Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 233-6795

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

SAMPLE EARLY COORDINATION LETTER

August 31, 2017

{See Attached List}

Re: Designation Number 1401828
Interchange Modification
I-69 at S.R. 14/Illinois Rd.
Aboite Township, Allen County, Indiana

Dear Interested Agency:

Indiana Department of Transportation (INDOT) proposes to utilize federal funds to modify an interchange located at Interstate (I) 69, and State Road (S.R.) 14/ Illinois Rd. in Aboite Township, Allen County, Indiana. This letter is part of the early coordination phase of the environmental review process. We request comments from your area of expertise regarding any possible environmental effects associated with this project. Please use the above designation number (Des. No.) and description in your reply. We will incorporate your comments into a study of the project's environmental impacts.

PROJECT LOCATION

The proposed project is located at I-69 and S.R 14/Illinois Rd in Allen County. Specifically, the project is in Sections 1 and 12, Township 30 North, Range 1 East and Sections 6 and 7, Township 30 North, Range 12 East, of the Fort Wayne West, Indiana 7.5 minute United States Geological Survey (USGS) topographic quadrangle.

EXISTING CONDITIONS

I-69 is classified as an Interstate Highway, with a posted speed limit of 65 miles per hour (mph) and average daily traffic (ADT) of 70,395 and 50,047 VPD north and south of Illinois Rd. respectively. The road consists of six 12 ft. wide concrete lanes, with 12 and 14 ft. wide outer and median shoulders. A concrete median barrier and underdrain pipes are located within the project area.

S.R. 14/Illinois Rd. is classified as a Principal Arterial, with a posted speed limit of 45 mph and ADT of 30,749 and 37,938 vehicles per day west and east of I-69 respectively. The road consists of four 12 ft. wide asphalt lanes, with a 10 ft. wide outside shoulder and 2 ft. curbed median shoulder. Concrete barriers are provided in each direction.

The existing interchange is a full cloverleaf at S.R. 14/Illinois road with loop radii of 208.4 ft. Current traffic counts show a combined AM peak-hour volume of 1,800 vehicles per day (VPD). I-69 intersects S.R. 14/Illinois Rd. at approximately 83 degrees. Both roadways are generally level in alignment.

Land use in the project area consists of residential and commercial.

Although utilities likely exist in the project area, the types, owners, and exact locations have not yet been determined. Coordination with utilities is being conducted by the designer and will be ongoing through the duration of the project.

PROJECT NEED AND PURPOSE

The need for this project is evidenced by a weaving conflict between the southwest and southeast I-69 off-ramp loops and eastbound through-traffic on Illinois Road. Drivers have reported confusion about how to legally merge and who should yield to whom. American Association of State Highway and Transportation Officials (AASHTO) guidance recommends against adjacent loops when the sum of the traffic volumes on the two ramps exceeds 1000 vehicles per



hour (vph) because of the effect of weaving on through-traffic. Current INDOT traffic counts show a combined morning peak-hour volume of nearly 1,800 vph on southwest and southeast I-69 off-ramp loops.

The purpose of this project is to provide a safe roadway for motorists

PROPOSED IMPROVEMENTS

The proposed improvements consist of closing the southwest I-69 off-ramp loop and routing that traffic onto the northwest I-69 off-ramp loop. The median barrier will be removed at the location. Two left turn lanes will be constructed in addition to the two existing right-turn lanes on the northwest I-69 off-ramp loop and the eastbound segment of S.R. 14/Illinois Rd. from the southwest I-69 off ramp loop to the bridge will be expanded to three lanes.

RIGHT-OF-WAY

No permanent or temporary right-of-way will be required for this project.

MAINTENANCE OF TRAFFIC

Maintenance of traffic will utilize phased construction with shoulder closures and temporary lane closures.

HISTORIC RESOURCES

Metric reviewed the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and identified 43 County Survey Sites within the 0.5 mile search radius. The nearest site is located approximately 0.05 mile northwest of the project area. No impact is expected.

Based on the scope of work, this project appears to fall under Category A-3 of the *Programmatic Agreement among the Federal Highway Administration, INDOT, the Advisory Council on Historic Preservation, and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana* (MPPA).

EARLY COORDINATION

As part of our early coordination effort for the proposed project, please study the enclosed information and provide a written evaluation of any identified potential impacts upon resources that are under your jurisdiction. It is requested, that you return a reply within 30-days of receipt of this letter. Should we not receive your response within 30 days, it will be assumed that your agency believes there will be no adverse effects incurred as a result of this project.

Your cooperation in expediting the development of the referenced project is appreciated. If you have any questions, or if we can be of any further assistance, please do not hesitate to contact this office.

On behalf of INDOT,
Metric Environmental, LLC



Ryan Hennessey
Environmental Scientist

cc: File No. 16-0108-3
Marc Rape, Strand Associates
Susan Doell, INDOT Project Manager

Attachments: Location Map, USGS Topographic Map, Aerial Map, Site Photographs, Soil Survey Map, National Wetlands Inventory Map, Flood Insurance Rate Map

Attachments removed for space conservation. See Appendices B and F.

Early Coordination
Des. No. 1401828, Interchange Modification
I-69 at S.R. 14/Illinois Rd.
Aboite Township, Allen County, Indiana

May 8, 2018

City of Fort Wayne Department of Public Works
Dawn Ritchie, Greenways Manager
{dawn.ritchie@cityoffortwayne.org}

April 12, 2018

Northeastern Indiana Regional Coordinating Council
{dan.avery@co.allen.in.us}

City of Fort Wayne Parks and Recreation
Chuck Reddinger, Deputy Director of Recreation
{chuck.reddinger@cityoffortwayne.org}

August 31, 2017

Natural Resources Conservation Service
{rick.neilson@in.usda.gov}

Indiana Geological Survey
{https://igs.indiana.edu/eAssessment/}

Indiana Department of Transportation
Office of Aviation
{AFrench2@indot.in.gov}

National Parks Service
Midwest Regional Office
{Tokey_Boswell@nps.gov}

Indiana Department of Natural Resources Division of
Fish and Wildlife
{environmentalreview@dnr.in.gov}

U.S. Department of Housing & Urban Develop.
Chicago Regional Office
{Melanie.H.Castillo@hud.gov}

U.S. Fish and Wildlife Service
Bloomington Field Office
{Elizabeth_McCloskey@fws.gov}

Allen County Surveyor
Jeff Sorg
{Surveyor@co.allen.in.us}

Indiana Department of Environmental
Management
Proposed Roadway Construction Projects Letter
{http://www.in.gov/idem/5284.htm}

IDEM Wellhead Proximity Determinator
Electronic Review of Location
{http://www.in.gov/idem/cleanwater/pages/
we llhead/}

Indiana Department of Transportation
{rclark@indot.in.gov}

U.S. Army Corps of Engineers
Louisville District
{Gregory.A.McKay@usace.army.mil}

Allen County Highway Department
Mike Thornson, County Engineer
{mike.thornson@co.allen.in.us}

Allen County Commissioners
{commissioner@co.allen.in.us}

Northeastern Indiana Regional Coordinating
Council
{dan.avery@co.allen.in.us}

City of Fort Wayne Parks and Recreation Chuck
Reddinger, Deputy Director of Recreation
{chuck.reddinger@cityoffortwayne.org}



INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 232-XXXX
FAX: (317) 233-XXXX

Eric Holcomb, Governor
Joe McGuinness, Commissioner

August 31, 2017

TO: MS4 COORDINATOR
Allen County
200 E Berry Street Ste 350
Fort Wayne, IN 46802

FROM: Ryan Hennessey
Metric Environmental, LLC
6971 Hillsdale Ct.
Indianapolis, IN 46250

RE: Early Notification
INDOT DES Number: 1401828
Location: I-69 at S.R. 14/Illinois Rd.
Description: Interchange Modification

The Indiana Department of Transportation (INDOT) and Federal Highway Administration (FHWA) intend to proceed with the above project. You are being notified because this project lies within an Urbanized Area Boundary (UAB). In accordance with 327 IAC 15-13 (Rule 13 - Municipal Separate Storm Sewer Systems), INDOT has developed a Storm Water Quality Management Plan (SWQMP).

As part of its implementation, projects falling within the UAB will be required to consider appropriate post construction storm water quality best management practices (BMPs). These BMPs should take into consideration the available space, pollutants of concern and receiving waters.

This letter is for notification purposes only, and no action is required by you; however, if you would like to provide your input on water quality concerns, please provide this information **within thirty (30) calendar days** from the date of this letter to the undersigned. Should we not receive your response within the specified timeframe, it will be assumed that your agency does not have additional concerns about water quality issues resulting from the proposed project. Should you find that an extension to the response time is necessary, a reasonable amount of time may be granted upon request. If you have any questions regarding this matter, please feel free to contact Ryan Hennessey at 317-608-2798. Thank you in advance for your attention to this matter.

Sincerely,
Ryan Hennessey
Environmental Geologist
Metric Environmental, LLC

U. S. Fish and Wildlife Service Interim Policy
For the
Review of Highway Transportation Projects in Indiana
5/29/2013

The intent of this interim policy is to make early coordination more efficient by reducing and streamlining the flow of early coordination between USFWS, INDOT and other coordinating agencies. The potential to impact wildlife habitat is the guiding criteria on when and how coordination should be initiated with the USFWS. This policy is intended to fulfill legal requirements for coordination under Section 7 of the Endangered Species Act (ESA). This policy can be used by all state, local and/or county highway agencies within the State of Indiana to fulfill early coordination with the USFWS.

This interim policy is an excerpt from the 09-27-2010 Draft Programmatic Agreement developed between INDOT and the USFWS and has been modified to reflect our current policy direction for the review of highway transportation projects in Indiana.

Classification of Coordination Types with U.S. Fish & Wildlife Service

There will be two types of coordination with the USFWS.

A. Programmatic Coordination

If construction activities meet the criteria established below, the USFWS agrees that the potential for impacts from these types of projects is minimal. In these cases, "**Programmatic Coordination**" constitutes the USFWS's early coordination roll, including Section 7 consultation requirements of the ESA of 1973, as amended. For these projects, no submittal of information to the USFWS is necessary. However, the standard recommendations as listed in Appendix A of this document should be included in the NEPA document. Only those projects that meet ALL of the following criteria qualify for programmatic coordination. If it is unclear whether the project fits under the below criteria, contact the appropriate USFWS office for clarification.

1. The project impacts less than 0.5 acre of forested R/W (temporary and/or permanent), all of which is within 75 feet of the edge of the existing roadway or pavement.
2. The project impacts less than 300 feet of natural perennial and intermittent streams without relocation, with the following exceptions:
 - a. Assumed non-jurisdictional roadway ditches.
 - b. Assumed jurisdictional waterways in disturbed areas where no wooded riparian habitat exists such as maintained legal and/or agricultural drains or waterways within residential, commercial and/or urban areas.
3. The project impacts less than 0.1 acre of wetlands, including both permanent and temporary impacts.
4. The project does not occur in the streams listed in Appendix B.
5. The project does not occur in the National Lakeshore in Lake, Porter, and LaPorte Counties
6. The project does not impact a **surface karst feature** within the Karst region of Indiana as discussed in Appendix C.

7. The project does not impact any natural area or wildlife habitat protected under the Federal Highway Administration's Section 4(f) Evaluation (49 USC Section 303, 23CFR 774) and/or Section 6(f) (16 USC Section 4601-F) of the Land and Water Conservation Fund Act.

B. Full Coordination

For all projects which do not qualify for programmatic coordination, an early coordination letter should be provided to the USFWS describing the entire project and its impacts to wildlife habitats within the project's impact area. If a response letter is deemed necessary, the USFWS will provide it within 30 days of receipt of the early coordination letter; otherwise, after 30 days, the preparer will incorporate the standard recommendations listed in Appendix A of this guidance into the NEPA document and Section 7 requirements will be considered fulfilled. In these cases the USFWS has determined that the project does not require a site-specific response.

If the USFWS requests an extension of time to provide their response, a reasonable extension of time shall be given, if possible.

Section 7 Evaluation

The USFWS concurs that projects that qualify for Programmatic Coordination or receive no USFWS response to full coordination have an effect determination of "Not Likely to Adversely Affect" any Endangered or Threatened species. This determination will satisfy requirements under the authority of the ESA of 1973, as amended.

Project Impact Modifications

If, during the development of the proposed project, changes occur that result in exceeding any of the criteria listed in Section A, or additional impacts are identified that could affect a threatened or endangered species, the project should not advance until full coordination is conducted with the USFWS.

If new information becomes available concerning federally listed species, proposed species, or other significant fish and wildlife resources, which might preclude the use of this interim policy, or require that the policy be amended (e. g. new counties or waterways be added), it will be the responsibility of the USFWS to inform INDOT as soon as possible. If INDOT staff discovers that such changes may affect a project that has already completed consultation, INDOT should notify the USFWS to reinstate Section 7 consultation.

Appendix A

Standard Recommendations

1. Do not clear trees or understory vegetation outside the construction zone boundaries. **(This restriction is not related to the “tree clearing” restriction for potential Indiana Bat habitat.)**
2. Restrict below low-water work in streams to placement of culverts, piers, pilings and/or footings, shaping of the spill slopes around the bridge abutments, and placement of riprap.

Culverts should span the active stream channel, should be either embedded or a 3-sided or open-arch culvert, and be installed where practicable on an essentially flat slope. When an open-bottomed culvert or arch is used in a stream, which has a good natural bottom substrate, such as gravel, cobbles and boulders, the existing substrate should be left undisturbed beneath the culvert to provide natural habitat for the aquatic community.

3. Restrict channel work and vegetation clearing to the minimum necessary for installation of the stream crossing structure.
4. Minimize the extent of hard armor (riprap) in bank stabilization by using bioengineering techniques whenever possible. If rip rap is utilized for bank stabilization, extend it below low-water elevation to provide aquatic habitat.
5. Implement temporary erosion and sediment control methods within areas of disturbed soil. All disturbed soil areas upon project completion will be vegetated following INDOT's standard specifications.
6. Avoid all work within the inundated part of the stream channel during the fish spawning season (April 1 through June 30); except for work within sealed structures such as caissons or cofferdams that were installed prior to the spawning season. No equipment shall be operated below Ordinary High Water Mark during this time unless the machinery is within the caissons or on the cofferdams.
7. Evaluate wildlife crossings under bridge/culverts projects in appropriate situations. Suitable crossings include flat areas below bridge abutments with suitable ground cover, high water shelves in culverts, amphibian tunnels and diversion fencing.

IDEM (<http://www.in.gov/idem/index.htm>) > Proposed Roadway Letter



Indiana Department of Environmental Management

We Protect Hoosiers and Our Environment.

100 North Senate Avenue - Indianapolis, IN 46204
(800) 451-6027 - (317) 232-8603 - www.idem.IN.gov

INDOT
Susan Doell
5333 Hatfield Rd
Fort Wayne , IN 46808
Date

Metric Environmental, LLC
Ryan Hennessey
6971 Hillsdale Ct
Indianapolis , IN 46250

Dear Grant Administrator or Other Finance Approval Authority:

RE: The need for this project is evidenced by a weaving conflict between the southwest and southeast I-69 off-ramp loops and eastbound through-traffic on Illinois Road. Drivers have reported confusion about how to legally merge and who should yield to whom. American Association of State Highway and Transportation Officials (AASHTO) guidance recommends against adjacent loops when the sum of the traffic volumes on the two ramps exceeds 1000 vehicles per hour (vph) because of the effect of weaving on through-traffic. Current INDOT traffic counts show a combined morning peak-hour volume of nearly 1,800 vph on southwest and southeast I-69 off-ramp loops. The proposed improvements consist of closing the southwest I-69 off-ramp loop and routing that traffic onto the northwest I-69 off-ramp loop. The median barrier will be removed at the location. Two left turn lanes will be constructed in addition to the two existing right-turn lanes on the northwest I-69 off-ramp loop and the eastbound segment of S.R. 14/Illinois Rd. from the southwest I-69 off ramp loop to the bridge will be expanded to three lanes.

The Indiana Department of Environmental Management (IDEM) is aware that many local government or not-for-profit entities are seeking grant monies, a bond issuance, or another public funding mechanism to cover some portion of the cost of a public works, infrastructure, or community development project. IDEM also is aware that in order to be eligible for such funding assistance, applicants are required to first evaluate the potential impacts that their particular project may have on the environment. In order to assist applicants seeking such financial assistance and to ensure that such projects do not have an adverse impact on the environment, IDEM has prepared the following list of environmental issues that each applicant must consider in order to minimize environmental impacts in compliance with all relevant state laws.

IDEM recommends that each applicant consider the following issues when moving forward with their project. IDEM also requests that, in addition to submitting the information requested above, each applicant also sign the attached certification, attesting to the fact that they have read the letter in its entirety, agree to abide by the recommendations of the letter, and to apply for any permits required from IDEM for the completion of their project.

IDEM recommends that any person(s) intending to complete a public works, infrastructure, or community development project using any public funding consider each of the following applicable recommendations and requirements:

WATER AND BIOTIC QUALITY

1. Section 404 of the Clean Water Act requires that you obtain a permit from the U.S. Army Corps of Engineers (USACE) before discharging dredged or fill materials into any wetlands or other waters, such as rivers, lakes, streams, and ditches. Other activities regulated include the relocation, channelization, widening, or other such alteration of a stream, and the mechanical clearing (use of heavy construction equipment) of wetlands. Thus, as a project owner or sponsor, it is your responsibility to ensure that no wetlands are disturbed without the proper permit. Although you may initially refer to the U.S. Fish and Wildlife Service National Wetland Inventory maps as a means of identifying potential areas of concern, please be mindful that those maps do not depict jurisdictional wetlands regulated by the USACE or the Department of Environmental Management. A valid jurisdictional wetlands determination can only be made by the USACE, using the 1987 Wetland Delineation Manual.

USACE recommends that you have a consultant check to determine whether your project will abut, or lie within, a wetland area. To view a list of consultants that have requested to be included on a list posted by the USACE on their Web site, see USACE Permits and Public Notices (<http://www.lrl.usace.army.mil/orf/default.asp>) (<http://www.lrl.usace.army.mil/orf/default.asp>) and then click on "Information" from the menu on the right-hand side of that page. Their "Consultant List" is the fourth entry down on the "Information" page. Please note that the USACE posts all consultants that request to appear on the list, and that inclusion of any particular consultant on the list does not represent an endorsement of that consultant by the USACE, or by IDEM.

Much of northern Indiana (Newton, Lake, Porter, LaPorte, St. Joseph, Elkhart, LaGrange, Steuben, and Dekalb counties; large portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and lesser portions of Benton, White, Pulaski, Kosciusko, and Wells counties) is served by the USACE District Office in Detroit (313-226-6812). The central and southern portions of the state (large portions of Benton, White, Pulaski, Kosciusko, and Wells counties; smaller portions of Jasper, Starke, Marshall, Noble, Allen, and Adams counties; and all other Indiana counties located in north-central, central, and southern Indiana) are served by the USACE Louisville District Office (502-315-6733).

Additional information on contacting these U.S. Army Corps of Engineers (USACE) District Offices, government agencies with jurisdiction over wetlands, and other water quality issues, can be found at <http://www.in.gov/idem/4396.htm> (<http://www.in.gov/idem/4396.htm>). IDEM recommends that impacts to wetlands and other water resources be avoided to the fullest extent.

2. In the event a Section 404 wetlands permit is required from the USACE, you also must obtain a Section 401 Water Quality Certification from the IDEM Office of Water Quality. To learn more about the water quality certification program, visit: <http://www.in.gov/idem/4384.htm> (<http://www.in.gov/idem/4384.htm>).
3. If the USACE determines that a wetland or other body of water is isolated and not subject to Clean Water Act regulation, it is still regulated by the state of Indiana. A state isolated wetland permit from IDEM's Office of Water Quality is required for any activity that results in the discharge of dredged or fill materials into isolated wetlands. To learn more about isolated wetlands, contact the Office of Water Quality at 317-233-8488.

4. If your project will impact more than 0.5 acres of wetland, stream relocation, or other large-scale alterations to bodies of water such as the creation of a dam or a water diversion, you should seek additional input from the Office of Water Quality, Wetlands staff at 317-233-8488.
5. Work within the one-hundred year floodway of a given body of water is regulated by the Department of Natural Resources, Division of Water. Contact this agency at 317-232-4160 for further information.
6. The physical disturbance of the stream and riparian vegetation, especially large trees overhanging any affected water bodies should be limited to only that which is absolutely necessary to complete the project. The shade provided by the large overhanging trees helps maintain proper stream temperatures and dissolved oxygen for aquatic life.
7. For projects involving construction activity (which includes clearing, grading, excavation and other land disturbing activities) that result in the disturbance of one (1), or more, acres of total land area, contact the Office of Water Quality – Watershed Planning Branch (317/233-1864) regarding the need for of a Rule 5 Storm Water Runoff Permit. Visit the following Web page
 - <http://www.in.gov/idem/4902.htm> (<http://www.in.gov/idem/4902.htm>)

To obtain, and operate under, a Rule 5 permit you will first need to develop a Construction Plan (<http://www.in.gov/idem/4917.htm#constreq>) (<http://www.in.gov/idem/4917.htm#constreq>), and as described in 327 IAC 15-5-6.5 (<http://www.in.gov/legislative/iac/T03270/A00150> [PDF] (<http://www.in.gov/legislative/iac/T03270/A00150.PDF>), pages 16 through 19). Before you may apply for a Rule 5 Permit, or begin construction, you must submit your Construction Plan to your county Soil and Water Conservation District (SWCD) (<http://www.in.gov/isda/soil/contacts/map.html> (<http://www.in.gov/isda/soil/contacts/map.html>)).

Upon receipt of the construction plan, personnel of the SWCD or the Indiana Department of Environmental Management will review the plan to determine if it meets the requirements of 327 IAC 15-5. Plans that are deemed deficient will require re-submittal. If the plan is sufficient you will be notified and instructed to submit the verification to IDEM as part of the Rule 5 Notice of Intent (NOI) submittal. Once construction begins, staff of the SWCD or Indiana Department of Environmental Management will perform inspections of activities at the site for compliance with the regulation.

Please be mindful that approximately 149 Municipal Separate Storm Sewer System (MS4) areas are now being established by various local governmental entities throughout the state as part of the implementation of Phase II federal storm water requirements. All of these MS4 areas will eventually take responsibility for Construction Plan review, inspection, and enforcement. As these MS4 areas obtain program approval from IDEM, they will be added to a list of MS4 areas posted on the IDEM Website at: <http://www.in.gov/idem/4900.htm> (<http://www.in.gov/idem/4900.htm>).

If your project is located in an IDEM-approved MS4 area, please contact the local MS4 program about meeting their storm water requirements. Once the MS4 approves the plan, the NOI can be submitted to IDEM.

Regardless of the size of your project, or which agency you work with to meet storm water requirements, IDEM recommends that appropriate structures and techniques be utilized both during the construction phase, and after completion of the project, to minimize the impacts associated with storm water runoff. The use of appropriate planning and site development and appropriate storm water quality measures are recommended to prevent soil from leaving the construction site during active land disturbance and for post construction water quality concerns. Information and assistance regarding storm water related to construction activities are available from the Soil and Water Conservation District (SWCD) offices in each county or from IDEM.

8. For projects involving impacts to fish and botanical resources, contact the Department of Natural Resources - Division of Fish and Wildlife (317-232-4080) for additional project input.
9. For projects involving water main construction, water main extensions, and new public water supplies, contact the Office of Water Quality - Drinking Water Branch (317-308-3299) regarding the need for permits.
10. For projects involving effluent discharges to waters of the State of Indiana , contact the Office of Water Quality - Permits Branch (317-233-0468) regarding the need for a National Pollutant Discharge Elimination System (NPDES) permit.
11. For projects involving the construction of wastewater facilities and sewer lines, contact the Office of Water Quality - Permits Branch (317-232-8675) regarding the need for permits.

AIR QUALITY

The above-noted project (see page 1) should be designed to minimize any impact on ambient air quality in, or near, the project area. The project must comply with all federal and state air pollution regulations. Consideration should be given to the following:

1. Regarding open burning, and disposing of organic debris generated by land clearing activities; some types of open burning are allowed under specific conditions (<http://www.in.gov/idem/4148.htm> (<http://www.in.gov/idem/4148.htm>)). You also can seek an open burning variance from IDEM.

IDEM generally recommends that you take vegetative wastes to a registered yard waste composting facility or that the waste be chipped or shredded with composting on-site. You must register with IDEM if more than 2,000 pounds is to be composted; contact 317-232-0066). The finished compost can then be used as a mulch or soil amendment. You also may bury any vegetative wastes (such as leaves, twigs, branches, limbs, tree trunks and stumps) on-site, although burying large quantities of such material can lead to subsidence problems.

2. Reasonable precautions must be taken to minimize fugitive dust emissions from construction and demolition activities. For example, wetting the area with water, constructing wind barriers, or treating dusty areas with chemical stabilizers (such as calcium chloride or several other commercial products). Dirt tracked onto paved roads from unpaved areas should be minimized.

If construction or demolition is conducted in a wooded area where blackbirds have roosted or abandoned buildings or building sections in which pigeons or bats have roosted for three to five years, precautionary measures should be taken to avoid an outbreak of histoplasmosis. This disease is caused by the fungus *Histoplasma capsulatum*, which stems from bird or bat droppings that have accumulated in one area for three to five years. The spores from this fungus become airborne when the area is disturbed and can cause infections over an entire community downwind of the site. The area should be wetted down prior to cleanup or demolition of the project site. For more detailed information on histoplasmosis prevention and control, please contact the Acute Disease Control Division of the Indiana State Department of Health at 317-233-7272.

3. The U.S. EPA and the U.S. Surgeon General recommend that people not have long-term exposure to radon at levels above 4 pCi/L. For a county-by-county map of predicted radon levels in Indiana , visit <http://www.in.gov/idem/4267.htm> (<http://www.in.gov/idem/4267.htm>).

The U.S. EPA further recommends that all homes and apartments (within three stories of ground level) be tested for radon. If in-home radon levels are determined to be 4 pCi/L or higher, then U.S. EPA recommends a follow-up test. If the second test confirms that radon levels are 4 pCi/L or higher, then U.S. EPA recommends the installation of radon-reduction measures. For a list of qualified radon testers and radon mitigation (or reduction) specialists, visit http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf (http://www.in.gov/isdh/regsvcs/radhealth/pdfs/radon_testers_mitigators_list.pdf). Also, it is recommended that radon reduction measures be built into all new homes, particularly in areas like Indiana that have moderate to high predicted radon levels.

To learn more about radon, radon risks, and ways to reduce exposure, visit <http://www.in.gov/isdh/regsvcs/radhealth/radon.htm> (<http://www.in.gov/isdh/regsvcs/radhealth/radon.htm>), <http://www.in.gov/idem/4145.htm> (<http://www.in.gov/idem/4145.htm>), or <http://www.epa.gov/radon/index.html> (<http://www.epa.gov/radon/index.html>).

4. With respect to asbestos removal, all facilities slated for renovation or demolition (except residential buildings that have four (4) or fewer dwelling units and which will not be used for commercial purposes) must be inspected by an Indiana-licensed asbestos inspector prior to the commencement of any renovation or demolition activities. If regulated asbestos-containing material (RACM) that may become airborne is found, any subsequent demolition, renovation, or asbestos removal activities must be performed in accordance with the proper notification and emission control requirements.

If no asbestos is found where a renovation activity will occur, or if the renovation involves removal of less than 260 linear feet of RACM off of pipes, less than 160 square feet of RACM off of other facility components, or less than 35 cubic feet of RACM off of all facility components, the owner or operator of the project does not need to notify IDEM before beginning the renovation activity.

For questions on asbestos demolition and renovation activities, you can also call IDEM's Lead/Asbestos section at 1-888-574-8150.

In all cases where a demolition activity will occur (even if no asbestos is found), the owner or operator must still notify IDEM 10 working days prior to the demolition, using the form found at www.in.gov/icpr/webfile/formsdiv/44593.pdf.

Anyone submitting a renovation/demolition notification form will be billed a notification fee based upon the amount of friable asbestos containing material to be removed or demolished. Projects that involve the removal of more than 2,600 linear feet of friable asbestos containing materials on pipes, or 1,600 square feet or 400 cubic feet of friable asbestos containing material on other facility components, will be billed a fee of \$150 per project; projects below these amounts will be billed a fee of \$50 per project. Billings will occur on a quarterly basis.

For more information about IDEM policy regarding asbestos removal and disposal, visit: <http://www.in.gov/idem/4983.htm> (<http://www.in.gov/idem/4983.htm>).

5. With respect to lead-based paint removal, IDEM encourages all efforts to minimize human exposure to lead-based paint chips and dust. IDEM is particularly concerned that young children exposed to lead can suffer from learning disabilities. Although lead-based paint abatement efforts are not mandatory, any abatement that is conducted within housing built before January 1, 1978, or a child-occupied facility is required to comply with all lead-based paint work practice standards, licensing and notification requirements. For more information about lead-based paint removal, visit <http://www.in.gov/idem/permits/guide/waste/leadabatement.html> (<http://www.in.gov/idem/permits/guide/waste/leadabatement.html>).
6. Ensure that asphalt paving plants are permitted and operate properly. The use of cutback asphalt, or asphalt emulsion containing more than seven percent (7%) oil distillate, is prohibited during the months of April through October. See 326 IAC 8-5-2, Asphalt Paving Rule (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>) (<http://www.ai.org/legislative/iac/T03260/A00080.PDF>).
7. If your project involves the construction of a new source of air emissions or the modification of an existing source of air emissions or air pollution control equipment, it will need to be reviewed by the IDEM Office of Air Quality (OAQ). A registration or permit may be required under 326 IAC 2 (www.ai.org/legislative/iac/t03260/a00020.pdf) (<http://www.ai.org/legislative/iac/t03260/a00020.pdf>). New sources that use or emit hazardous air pollutants may be subject to Section 112 of the Clean Air Act and corresponding state air regulations governing hazardous air pollutants.
8. For more information on air permits, visit <http://www.in.gov/idem/4223.htm> (<http://www.in.gov/idem/4223.htm>), or to initiate the IDEM air permitting process, please contact the Office of Air Quality Permit Reviewer of the Day at (317) 233-0178 or oamprod@idem.in.gov.

LAND QUALITY

In order to maintain compliance with all applicable laws regarding contamination and/or proper waste disposal, IDEM recommends that:

1. If the site is found to contain any areas used to dispose of solid or hazardous waste, you need to contact the Office of Land Quality (OLQ) at 317-308-3103.
2. All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. For more information, visit <http://www.in.gov/idem/4998.htm> (<http://www.in.gov/idem/4998.htm>).
3. If any contaminated soils are discovered during this project, they may be subject to disposal as hazardous waste. Please contact the OLQ at 317-308-3103 to obtain information on proper disposal procedures.
4. If Polychlorinated Biphenyls (PCBs) are found at this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding management of any PCB wastes from this site.
5. If there are any asbestos disposal issues related to this site, please contact the Industrial Waste Section of OLQ at 317-308-3103 for information regarding the management of asbestos wastes. (Asbestos removal is addressed above, under Air Quality.)
6. If the project involves the installation or removal of an underground storage tank, or involves contamination from an underground storage tank, you must contact the IDEM Underground Storage Tank program at 317-308-3039 (<http://www.in.gov/idem/4999.htm>) (<http://www.in.gov/idem/4999.htm>).

FINAL REMARKS

Should the applicant need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that they notify all adjoining property owners and/or occupants within ten days of your submittal of each permit application. Applicants seeking multiple permits, may still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Please note that this letter does not constitute a permit, license, endorsement, or any other form of approval on the part of either the Indiana Department of Environmental Management or any other Indiana state agency.

Should you have any questions relating to the content or recommendations of this letter, or if you have additional questions about whether a more complete environmental review of your project should be conducted, please feel free to contact Steve Howell at (317) 232-8587, snhowell@idem.in.gov.

Signature(s) of the Applicant

I acknowledge that I am seeking grant monies, a bond issuance, or other public funding mechanism to cover some portion of the cost of the public works, infrastructure, or community development project as described herein, which I am working (possibly with others) to complete.

Project Description

The need for this project is evidenced by a weaving conflict between the southwest and southeast I-69 off-ramp loops and eastbound through-traffic on Illinois Road. Drivers have reported confusion about how to legally merge and who should yield to whom. American Association of State Highway and Transportation Officials (AASHTO) guidance recommends against adjacent loops when the sum of the traffic volumes on the two ramps exceeds 1000 vehicles per hour (vph) because of the effect of weaving on through-traffic. Current INDOT traffic counts show a combined morning peak-hour volume of nearly 1,800 vph on southwest and southeast I-69 off-ramp loops. The proposed improvements consist of closing the southwest I-69 off-ramp loop and routing that traffic onto the northwest I-69 off-ramp loop. The median barrier will be removed at the location. Two left turn lanes will be constructed in addition to the two existing right-turn lanes on the northwest I-69 off-ramp loop and the eastbound segment of S.R. 14/Illinois Rd. from the southwest I-69 off ramp loop to the bridge will be expanded to three lanes.

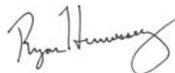
With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environmental Management that appears directly above. In addition, I understand that in order to complete the project in which I am interested, with a minimum impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Dated Signature of the Public Owner
Contact/Responsible Elected Official _____

Susan J. Doell,
P.E.

Digitally signed by Susan J. Doell, P.E.
DN: cn=Susan J. Doell, P.E., o=INDOT,
ou=Consultant Services,
email=sdoell@indot.in.gov, c=US
Date: 2017.08.31 15:31:20 -0400

Dated Signature of the Project
Planner/Consultant Contact Person _____

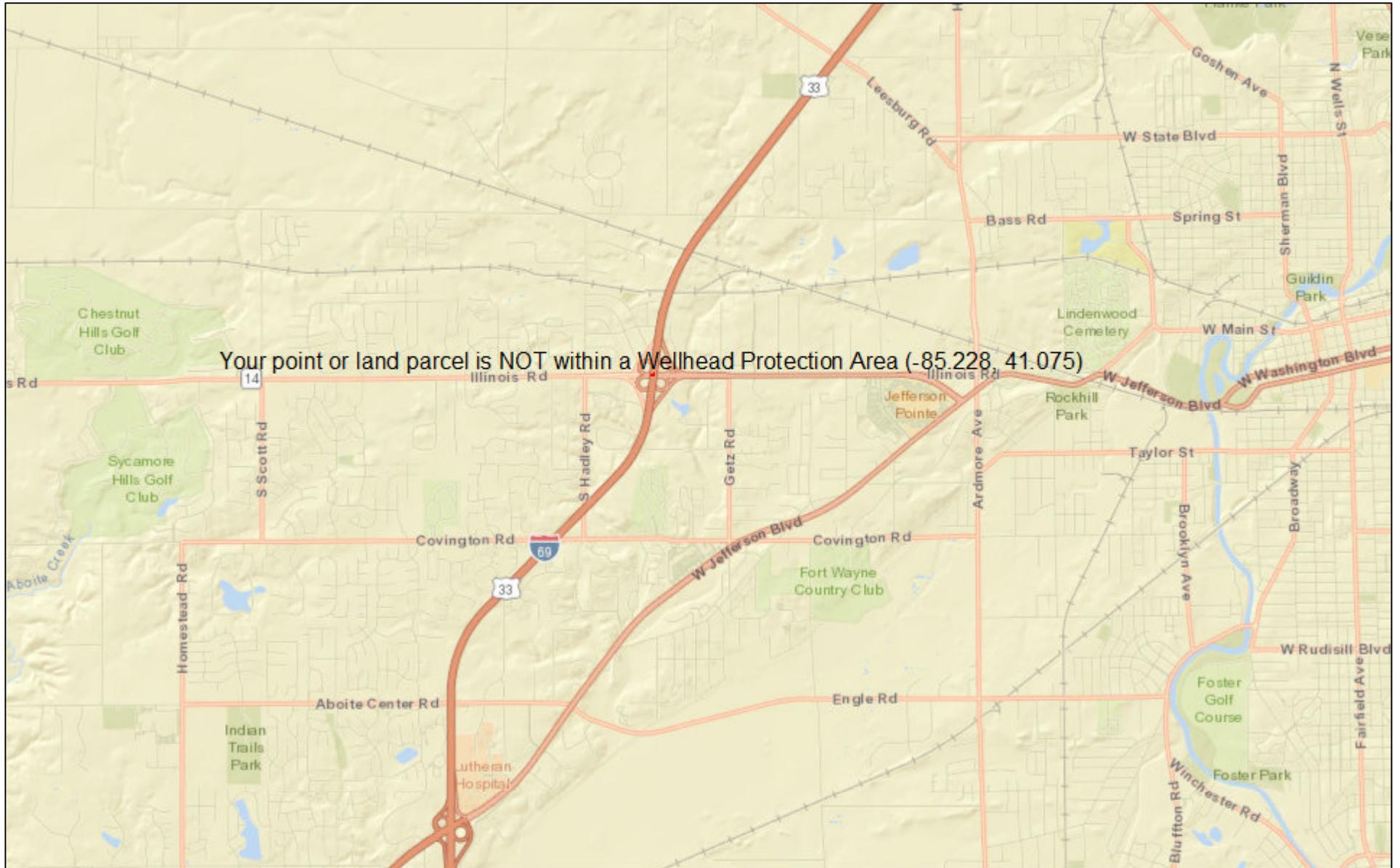


Susan Doell

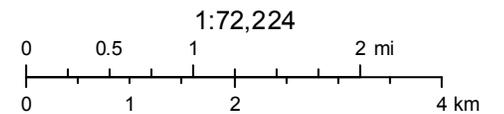
Ryan Hennessey

Indiana Department of Environmental Management

IDEM Wellhead Proximity



August 31, 2017



Indiana Department of Natural Resources
Sources: Esri, HERE, Garmin, USGS, Intermap, INCREMENT P, NRCan,



Organization and Project Information

Project ID: 16-0108-3
Des. ID: 1401828
Project Title: Interchange Modification
Name of Organization: Metric Environmental LLC
Requested by: Ryan Hennessey

Environmental Assessment Report

1. Geological Hazards:

- Moderate liquefaction potential
- Floodway

2. Mineral Resources:

- Bedrock Resource: High Potential
- Sand and Gravel Resource: Low Potential

3. Active or abandoned mineral resources extraction sites:

- None documented in the area

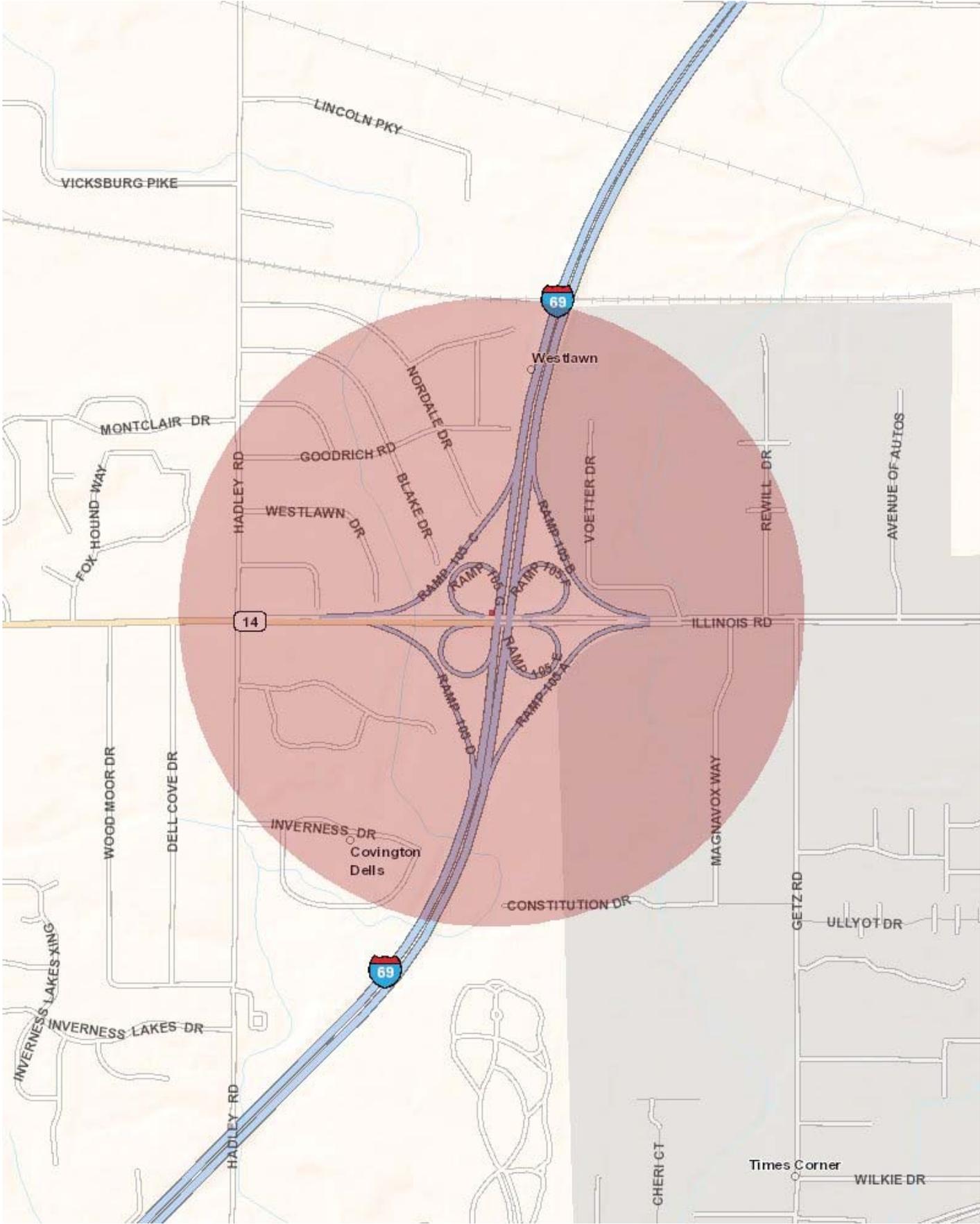
*All map layers from Indiana Map (maps.indiana.edu)

DISCLAIMER:

This document was compiled by Indiana University, Indiana Geological Survey, using data believed to be accurate; however, a degree of error is inherent in all data. This product is distributed "AS-IS" without warranties of any kind, either expressed or implied, including but not limited to warranties of suitability to a particular purpose or use. No attempt has been made in either the design or production of these data and document to define the limits or jurisdiction of any federal, state, or local government. The data used to assemble this document are intended for use only at the published scale of the source data or smaller (see the metadata links below) and are for reference purposes only. They are not to be construed as a legal document or survey instrument. A detailed on-the-ground survey and historical analysis of a single site may differ from these data and this document.

This information was furnished by Indiana Geological Survey
Address: 611 N. Walnut Grove Avenue, Bloomington, IN 47405-2208
Email: IGSEnvir@indiana.edu
Phone: 812 855-7428

Date: August 31, 2017





INDIANA DEPARTMENT OF TRANSPORTATION

100 North Senate Avenue
Room N955
Indianapolis, Indiana 46204

PHONE: (317) 232-1477
FAX: (317) 232-1499

Eric Holcomb, Governor
Joe McGuinness, Commissioner

September 15, 2017

Mr. Ryan Hennessey, Environmental Scientist
Metric Environmental
6971 Hillsdale Court
Indianapolis, IN 46250

Subject: Early Coordination Review (Des. No. 1401828)

Dear Mr. Hennessey,

In response to your request on August 31, 2017 for early coordination review of a project to modify an interchange located at Interstate 69, and State Road 14/ Illinois Rd. in Aboite Township, Allen County, Indiana; the Indiana Department of Transportation, Office of Aviation has reviewed the information and provides the following:

Are there any existing or proposed public-use airports within 5 nautical miles of the project limits (IC 8-21-10-6)?

The nearest public-use airport is located beyond five nautical miles of the proposed project site.

Will an Indiana Tall Structure permit (IC 8-21-10-3-a) and/or Noise Sensitive (IC 8-21-10-3-b) permit be required?

Based upon the provided information, an Indiana Tall Structure permit would not be required unless the project involves the construction of a temporary (e.g., crane) or permanent structure that exceeds 200 feet above ground level.

For any questions related to Indiana Tall Structure and/or Noise Sensitive permitting, please contact James Kinder at (317) 232-1485 jkinder2@indot.in.gov.

Sincerely,

Adam French, MPA
Chief Airport Inspector, Office of Aviation
Indiana Department of Transportation

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

DNR #: ER-20050

Request Received: August 31, 2017

Requestor: Metric Environmental
Ryan Hennessey
6971 Hillside Court
Indianapolis, IN 46250

Project: I-69 interchange modification at SR 14/Illinois Road, Fort Wayne; Des #1401828

County/Site info: Allen

The Indiana Department of Natural Resources has reviewed the above referenced project per your request. Our agency offers the following comments for your information and in accordance with the National Environmental Policy Act of 1969.

If our agency has regulatory jurisdiction over the project, the recommendations contained in this letter may become requirements of any permit issued. If we do not have permitting authority, all recommendations are voluntary.

Regulatory Assessment: This proposal may require the formal approval of our agency pursuant to the Flood Control Act (IC 14-28-1) for any proposal to construct, excavate, or fill in or on the floodway of Durnell Ditch, unless it qualifies for a bridge exemption (see enclosure). Please include a copy of this letter with the permit application, if required.

Natural Heritage Database: The Natural Heritage Program's data have been checked. To date, no plant or animal species listed as state or federally threatened, endangered, or rare have been reported to occur in the project vicinity.

Fish & Wildlife Comments: Avoid and minimize impacts to fish, wildlife, and botanical resources to the greatest extent possible, and compensate for impacts. We recommend a mitigation plan be developed (and submitted with the permit application, if required) if habitat impacts will occur. The DNR's Floodway Habitat Mitigation guidelines (and plant lists) can be found online at: <http://www.in.gov/legislative/iac/20140806-IR-312140295NRA.xml.pdf>.

Impacts to non-wetland forest of one (1) acre or more should be mitigated at a minimum 2:1 ratio. If less than one acre of non-wetland forest is removed in a rural setting, replacement should be at a 1:1 ratio based on area. Impacts to non-wetland forest under one (1) acre in an urban setting should be mitigated by planting five trees, at least 2 inches in diameter-at-breast height (dbh), for each tree which is removed that is 10" dbh or greater (5:1 mitigation based on the number of large trees).

The additional measures listed below should be implemented to avoid, minimize, or compensate for impacts to fish, wildlife, and botanical resources:

1. Revegetate all bare and disturbed areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub and hardwood tree species as soon as possible upon completion.
2. Do not excavate in the waterway and minimize disturbance to bank vegetation and contain disturbance to within the project limits.
3. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 3 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities) from April 1 through September 30.
4. Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized.

Attachments: A - Bridge Exemption Criteria

THIS IS NOT A PERMIT

State of Indiana
DEPARTMENT OF NATURAL RESOURCES
Division of Fish and Wildlife
Early Coordination/Environmental Assessment

5. Seed and protect disturbed stream banks that are 3:1 or steeper with heavy-duty net-free biodegradable erosion control blankets to minimize the entrapment and snaring of small wildlife such as snakes and turtles (follow manufacturer's recommendation for installation); seed and apply mulch on all other disturbed areas.

6. Do not excavate or place fill in any riparian wetland.

Contact Staff:

Christie L. Stanifer, Environ. Coordinator, Fish & Wildlife

Our agency appreciates this opportunity to be of service. Please contact the above staff member at (317) 232-4080 if we can be of further assistance.



Date: September 28, 2017

Christie L. Stanifer
Environ. Coordinator
Division of Fish and Wildlife

The Flood Control Act (IC 14-28-1) contains a provision (Section 22), which exempts certain bridge projects from its permitting requirement. Specifically, the Act states:

A permit is not required for "a construction or reconstruction project on a state or county highway bridge in a rural area that crosses a stream having an upstream drainage area of not more than fifty (50) square miles..."

Therefore, in order for a bridge project to be exempt, it must:

- be a state or county highway department project;
- be a bridge;
- be located in a rural area; and
- cross a stream having an upstream drainage area of less than 50 square miles.

The initial criterion is very specific - the structure must be a state or county highway department project.

The second requirement mandates that the project be a bridge (for this provision, the Department of Natural Resources considers a culvert to be a bridge). Projects such as bank protection, spoil disposal, borrow pits, etc. are not automatically exempt. Anyone proposing to undertake a non-bridge related activity should consult with the Division of Water's Technical Services Section staff at 317-232-4160 (or toll free at 1-877-928-3755) regarding the applicability of the exemption prior to initiating work.

The third criterion states that the project must be located in a rural area. The phrase "rural area" is defined as an area:

- where the lowest floor elevation, including a basement, of any residential, commercial, or industrial building impacted by the project is at least 2 feet above the 100 year flood elevation with the project in place;
- located outside the corporate boundaries of a consolidated or an incorporated city or town; and
- located outside of the territorial authority for comprehensive planning (generally, a 2 mile planning buffer around a city or town).

The final criterion limits the exemption to a project crossing a stream having an upstream drainage area of less than 50 square miles. The drainage area includes all land area contributing to runoff above the project site and is determined from the United States Geological Survey 7½ minute series quadrangle maps. The Department of Natural Resources will determine the drainage area upon written request.

This exemption has been grossly misunderstood and liberally applied in the past. As a result, the Department of Natural Resources is taking a firm stance on future violations. If challenged, it will be the responsibility of the person claiming the exemption to prove to the Department that all 4 criteria have been satisfied. Failure to do so will result in the Department initiating litigation with the potential for the imposition of fines in amounts up to \$10,000 per day.

Note: This exemption only applies to the Flood Control Act. If a bridge is to be constructed over a navigable waterway, or over or near a public freshwater lake, a permit will be required.

From: [McCloskey, Elizabeth](#)
To: [Ryan Hennessey](#)
Subject: Re: Des. No. 1401828, Interchange Modification; I-69 at S.R. 14/Illinois Rd; Aboite Township, Allen County, IN
Date: Wednesday, September 13, 2017 3:03:05 PM
Attachments: [image002.png](#)
[image003.png](#)
[image004.png](#)
[image001.png](#)
[image005.png](#)

Good afternoon Mr. Hennessey, because this project is entirely within the existing highway right-of-way and only affects developed land, with no natural resources or endangered species being impacted, the U.S. Fish and Wildlife Service will not be providing a letter.

Thank you.

Elizabeth McCloskey
U.S. Fish and Wildlife Service
Northern Indiana Suboffice

On Thu, Aug 31, 2017 at 10:54 AM, Ryan Hennessey <ryanh@metricenv.com> wrote:

Hi Ms. McCloskey,

Metric Environmental has been contracted by Strand Associates to prepare the Categorical Exclusion Environmental Document for the above referenced INDOT project.

We respectfully request your review of the attached early coordination packet and response within 30 days.

Thank you,

Ryan Hennessey
Project Scientist



Phone: 317.608.2798 Email: ryanh@metricenv.com
6971 Hillside Court, Indianapolis, IN 46250



www.metricenv.com

Complex Environment. Creative Solutions.

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Northeastern Indiana Regional Coordinating Council



October 4, 2017

Ryan Hennessey
Project Scientist
Metric Environmental
6971 Hillside Court
Indianapolis, IN 46250

Re: Early Coordination Des #1401828 I-69 at SR 14 Interchange Modification
Location: Aboite Township, Allen County, Indiana

Dear Mr. Hennessey:

Members of our staff reviewed your letter and report, dated August 31, 2017, concerning the Early Coordination of the I-69 at SR 14 Interchange Modification project. The NIRCC staff has the following comments relating to the early coordination phase of the environmental review process with this project, see below.

- There may be a NIPSCO natural gas pipeline running east/west near the southern half of the project area.

Thank you for the opportunity to comment on this project. If you have any questions, please do not hesitate to contact our office.

Sincerely,

A handwritten signature in cursive script that reads 'Stacey Gorsuch'.

Stacey Gorsuch
Principal Transportation Planner



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

In Reply Refer To:

February 12, 2018

Consultation Code: 03E12000-2018-SLI-0712

Event Code: 03E12000-2018-E-02331

Project Name: Des. No. 1401828, Interchange Modification, I-69 at S.R. 14, Allen County, IN

Subject: Updated list of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The attached species list identifies any federally threatened, endangered, proposed and candidate species that may occur within the boundary of your proposed project or may be affected by your proposed project. The list also includes designated critical habitat if present within your proposed project area or affected by your project. This list is provided to you as the initial step of the consultation process required under section 7(c) of the Endangered Species Act, also referred to as Section 7 Consultation.

Section 7 of the Endangered Species Act of 1973 requires that actions authorized, funded, or carried out by Federal agencies not jeopardize federally threatened or endangered species or adversely modify designated critical habitat. To fulfill this mandate, Federal agencies (or their designated non-federal representative) must consult with the Service if they determine their project “may affect” listed species or critical habitat.

Under 50 CFR 402.12(e) (the regulations that implement Section 7 of the Endangered Species Act) the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally. You may verify the list by visiting the ECOS-IPaC website <http://ecos.fws.gov/ipac/> at regular intervals during project planning and implementation and completing the same process you used to receive the attached list. As an alternative, you may contact this Ecological Services Field Office for updates.

Please use the species list provided and visit the U.S. Fish and Wildlife Service's Region 3 Section 7 Technical Assistance website at - <http://www.fws.gov/midwest/endangered/section7/s7process/index.html>. This website contains step-by-step instructions which will help you

determine if your project will have an adverse effect on listed species and will help lead you through the Section 7 process.

For all **wind energy projects** and **projects that include installing towers that use guy wires or are over 200 feet in height**, please contact this field office directly for assistance, even if no federally listed plants, animals or critical habitat are present within your proposed project or may be affected by your proposed project.

Although no longer protected under the Endangered Species Act, be aware that bald eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*) and Migratory Bird Treaty Act (16 U.S.C. 703 *et seq.*), as are golden eagles. Projects affecting these species may require measures to avoid harming eagles or may require a permit. If your project is near an eagle nest or winter roost area, see our Eagle Permits website at <http://www.fws.gov/midwest/midwestbird/EaglePermits/index.html> to help you determine if you can avoid impacting eagles or if a permit may be necessary.

We appreciate your concern for threatened and endangered species. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

(812) 334-4261

Project Summary

Consultation Code: 03E12000-2018-SLI-0712

Event Code: 03E12000-2018-E-02331

Project Name: Des. No. 1401828, Interchange Modification, I-69 at S.R. 14, Allen County, IN

Project Type: TRANSPORTATION

Project Description: INDOT Fort Wayne proposes to proceed with an intersection improvement project, located at I-69 and S.R. 14, Allen County, IN. The scope of work to be done includes closing the southwest I-69 off-ramp loop and routing that traffic onto the northwest I-69 off-ramp loop, removing the median barrier, adding two left turn lanes and two right turn lanes on the northwest I-69 off-ramp loop and expanding the eastbound segment of S.R. 14 to three lanes. The project extends from Reference Post (RP) 305+18 to RP 305+37 for a total of approximately 3000 feet along I-69 and 650 feet along S.R. 14. Land use within the project area consists of residential and forested. Suitable summer habitat is located in the northwest quadrant of the project area. No suitable summer habitat will be removed as a result of this project. This project is expected to Let during the March of 2020 and extend until September 2020. No permanent lighting will be added as a result of this project. Temporary lighting will not be used. The RFI dated, January 4, 2018, stated that a review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The bat inspection conducted by Metric Environmental on August 30, 2017 stated that no evidence of bats were seen or heard within the project area.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/place/41.07435280369525N85.22819979236469W>



Counties: Allen, IN

Endangered Species Act Species

There is a total of 3 threatened, endangered, or candidate species on this species list. Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Note that 1 of these species should be considered only under certain conditions. See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

Mammals

NAME	STATUS
Indiana Bat <i>Myotis sodalis</i> There is final critical habitat for this species. Your location is outside the critical habitat. Species profile: https://ecos.fws.gov/ecp/species/5949 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/1/office/31440.pdf	Endangered
Northern Long-eared Bat <i>Myotis septentrionalis</i> No critical habitat has been designated for this species. This species only needs to be considered under the following conditions: <ul style="list-style-type: none"> Incidental take of the NLEB is not prohibited here. Federal agencies may consult using the 4(d) rule streamlined process. Transportation projects may consult using the programmatic process. See www.fws.gov/midwest/endangered/mammals/nleb/index.html Species profile: https://ecos.fws.gov/ecp/species/9045 Species survey guidelines: https://ecos.fws.gov/ipac/guideline/survey/population/10043/office/31440.pdf	Threatened

Clams

NAME	STATUS
Rayed Bean <i>Villosa fabalis</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/5862	Endangered

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.



United States Department of the Interior



FISH AND WILDLIFE SERVICE

Indiana Ecological Services Field Office

620 South Walker Street

Bloomington, IN 47403-2121

Phone: (812) 334-4261 Fax: (812) 334-4273

<http://www.fws.gov/midwest/Endangered/section7/s7process/step1.html>

IPaC Record Locator: 718-11139037

February 12, 2018

Subject: Consistency letter for the 'Des. No. 1401828, Interchange Modification, I-69 at S.R. 14, Allen County, IN' project (TAILS 03E12000-2018-R-0712) under the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat.

To whom it may concern:

The U.S. Fish and Wildlife Service (Service) has received your request dated to verify that the **Des. No. 1401828, Interchange Modification, I-69 at S.R. 14, Allen County, IN** (Proposed Action) may rely on the revised February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects within the Range of the Indiana Bat and Northern Long-eared Bat (PBO) to satisfy requirements under Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat.884, as amended; 16 U.S.C. 1531 *et seq.*).

Based on the information you provided (Project Description shown below), you have determined that the Proposed Action will have no effect on the endangered Indiana bat (*Myotis sodalis*) or the threatened Northern long-eared bat (*Myotis septentrionalis*). If the Proposed Action is not modified, **no consultation is required for these two species.**

For Proposed Actions that include bridge/structure removal, replacement, and/or maintenance activities: If your initial bridge/structure assessments failed to detect Indiana bats, but you later detect bats during construction, please submit the Post Assessment Discovery of Bats at Bridge/Structure Form (User Guide Appendix E) to this Service Office. In these instances, potential incidental take of Indiana bats may be exempted provided that the take is reported to the Service.

If the Proposed Action may affect any other federally-listed or proposed species and/or designated critical habitat, additional consultation between the lead Federal action agency and this Service Office is required. If the proposed action has the potential to take bald or golden eagles, additional coordination with the Service under the Bald and Golden Eagle Protection Act

may also be required. In either of these circumstances, please advise the lead Federal action agency for the Proposed Action accordingly.

The following species may occur in your project area and **are not** covered by this determination:

- Rayed Bean, *Villosa fabalis* (Endangered)

Project Description

The following project name and description was collected in IPaC as part of the endangered species review process.

Name

Des. No. 1401828, Interchange Modification, I-69 at S.R. 14, Allen County, IN

Description

INDOT Fort Wayne proposes to proceed with an intersection improvement project, located at I-69 and S.R. 14, Allen County, IN. The scope of work to be done includes closing the southwest I-69 off-ramp loop and routing that traffic onto the northwest I-69 off-ramp loop, removing the median barrier, adding two left turn lanes and two right turn lanes on the northwest I-69 off-ramp loop and expanding the eastbound segment of S.R. 14 to three lanes. The project extends from Reference Post (RP) 305+18 to RP 305+37 for a total of approximately 3000 feet along I-69 and 650 feet along S.R. 14. Land use within the project area consists of residential and forested. Suitable summer habitat is located in the northwest quadrant of the project area. No suitable summer habitat will be removed as a result of this project. This project is expected to Let during the March of 2020 and extend until September 2020. No permanent lighting will be added as a result of this project. Temporary lighting will not be used. The RFI dated, January 4, 2018, stated that a review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. The bat inspection conducted by Metric Environmental on August 30, 2017 stated that no evidence of bats were seen or heard within the project area.

Determination Key Result

Based on the information you provided, you have determined that the Proposed Action will have no effect on the endangered Indiana bat and/or the threatened Northern long-eared bat. Therefore, no consultation with the U.S. Fish and Wildlife Service pursuant to Section 7(a)(2) of the Endangered Species Act of 1973 (ESA) (87 Stat. 884, as amended 16 U.S.C. 1531 *et seq.*) is required for these two species.

Qualification Interview

1. Is the project within the range of the Indiana bat^[1]?

[1] See [Indiana bat species profile](#)

Automatically answered

Yes

2. Is the project within the range of the Northern long-eared bat^[1]?

[1] See [Northern long-eared bat species profile](#)

Automatically answered

Yes

3. Which Federal Agency is the lead for the action?

A) Federal Highway Administration (FHWA)

4. Are *all* project activities limited to non-construction^[1] activities only? (examples of non-construction activities include: bridge/abandoned structure assessments, surveys, planning and technical studies, property inspections, and property sales)

[1] Construction refers to activities involving ground disturbance, percussive noise, and/or lighting.

No

5. Does the project include *any* activities that are **greater than** 300 feet from existing road/rail surfaces^[1]?

[1] Road surface is defined as the actively used [e.g. motorized vehicles] driving surface and shoulders [may be pavement, gravel, etc.] and rail surface is defined as the edge of the actively used rail ballast.

No

6. Does the project include *any* activities **within** 0.5 miles of an Indiana bat and/or NLEB hibernaculum^[1]?

[1] For the purpose of this consultation, a hibernaculum is a site, most often a cave or mine, where bats hibernate during the winter (see suitable habitat), but could also include bridges and structures if bats are found to be hibernating there during the winter.

No

7. Is the project located **within** a karst area?

No

8. Is there *any* suitable^[1] summer habitat for Indiana Bat or NLEB **within** the project action area^[2]? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] The action area is defined as all areas to be affected directly or indirectly by the Federal action and not merely the immediate area involved in the action (50 CFR Section 402.02). Further clarification is provided by the [national consultation FAQs](#).

Yes

9. Will the project remove *any* suitable summer habitat^[1] and/or remove/trim any existing trees **within** suitable summer habitat?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

No

10. Have presence/probable absence (P/A) summer surveys^{[1][2]} been conducted^{[3][4]} **within** the suitable habitat located within your project action area?

[1] See the Service's [summer survey guidance](#) for our current definitions of suitable habitat.

[2] Presence/probable absence summer surveys conducted within the fall swarming/spring emergence home range of a documented Indiana bat hibernaculum (contact local Service Field Office for appropriate distance from hibernacula) that result in a negative finding requires additional consultation with the local Service Field Office to determine if clearing of forested habitat is appropriate and/or if seasonal clearing restrictions are needed to avoid and minimize potential adverse effects on fall swarming and spring emerging Indiana bats.

[3] For projects within the range of either the Indiana bat or NLEB in which suitable habitat is present, and no bat surveys have been conducted, the transportation agency will assume presence of the appropriate species. This assumption of presence should be based upon the presence of suitable habitat and the capability of bats to occupy it because of their mobility.

[4] Negative presence/probable absence survey results obtained using the [summer survey guidance](#) are valid for a minimum of two years from the completion of the survey unless new information (e.g., other nearby surveys) suggest otherwise.

No

11. Does the project include activities **within documented Indiana bat habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry biangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

12. Does the project include activities **within documented NLEB habitat**^{[1][2]}?

[1] Documented roosting or foraging habitat – for the purposes of this consultation, we are considering documented habitat as that where Indiana bats and/or NLEB have actually been captured and tracked using (1) radio telemetry to roosts; (2) radio telemetry triangulation/triangulation to estimate foraging areas; or (3) foraging areas with repeated use documented using acoustics. Documented roosting habitat is also considered as suitable summer habitat within 0.25 miles of documented roosts.)

[2] For the purposes of this key, we are considering documented corridors as that where Indiana bats and/or NLEB have actually been captured and tracked to using (1) radio telemetry; or (2) treed corridors located directly between documented roosting and foraging habitat.

No

13. Does the project include maintenance of the surrounding landscape at existing facilities (e.g., rest areas, stormwater detention basins)?

No

14. Does the project include wetland or stream protection activities associated with compensatory wetland mitigation?

No

15. Does the project include slash pile burning?

No

16. Does the project include *any* bridge removal, replacement, and/or maintenance activities (e.g., any bridge repair, retrofit, maintenance, and/or rehabilitation work)?

Yes

17. Is there *any* suitable habitat^[1] for Indiana bat or NLEB **within** 1,000 feet of the bridge? (includes any trees suitable for maternity, roosting, foraging, or travelling habitat)

[1] See the Service's current [summer survey guidance](#) for our current definitions of suitable habitat.

Yes

18. Has a bridge assessment^[1] been conducted **within** the last 24 months^[2] to determine if the bridge is being used by bats?

[1] See [User Guide Appendix D](#) for bridge/structure assessment guidance

[2] Assessments must be completed no more than 2 years prior to conducting any work below the deck surface on all bridges that meet the physical characteristics described in the Programmatic Consultation, regardless of whether assessments have been conducted in the past. Due to the transitory nature of bat use, a negative result in one year does not guarantee that bats will not use that bridge/structure in subsequent years.

Yes

SUBMITTED DOCUMENTS

- *Bat Structure Assessment Form 8-30-17.pdf* <https://ecos.fws.gov/ipac/project/MFQHBUD6AJBXDN6TFRPAJCD4ZE/projectDocuments/11138923>

19. Did the bridge assessment detect *any* signs of bats roosting in/under the bridge (bats, guano, etc.)?

Note: There is a small chance bridge assessments for bat occupancy do not detect bats. Should a small number of bats be observed roosting on a bridge just prior to or during construction, such that take is likely to occur or does occur in the form of harassment, injury or death, the PBO requires the action agency to report the take. Report all unanticipated take within 2 working days of the incident to the USFWS. Construction activities may continue without delay provided the take is reported to the USFWS and is limited to 5 bats per project.

No

20. Will the bridge removal, replacement, and/or maintenance activities include installing new or replacing existing **permanent** lighting?

No

21. Does the project include the removal, replacement, and/or maintenance of *any* structure other than a bridge? (e.g., rest areas, offices, sheds, outbuildings, barns, parking garages, etc.)

No

22. Will the project involve the use of **temporary** lighting *during* the active season?

No

23. Will the project install new or replace existing **permanent** lighting?

No

24. Does the project include percussives or other activities (**not including tree removal/trimming or bridge/structure work**) that will increase noise levels above existing traffic/background levels?

Yes

25. Will the activities that use percussives (**not including tree removal/trimming or bridge/structure work**) and/or increase noise levels above existing traffic/background levels be conducted *during* the active season^[1]?

[1] Coordinate with the local Service Field Office for appropriate dates.

No

26. Are *all* project activities that are **not associated with** habitat removal, tree removal/trimming, bridge or structure removal, replacement, and/or maintenance, lighting, or use of percussives, limited to actions that DO NOT cause any stressors to the bat species, including as described in the BA/BO (i.e. activities that do not involve ground disturbance, percussive noise, temporary or permanent lighting, tree removal/trimming, nor bridge/structure activities)?

Examples: lining roadways, unlighted signage, rail road crossing signals, signal lighting, and minor road repair such as asphalt fill of potholes, etc.

No

27. Will the project raise the road profile **above the tree canopy**?

No

28. Are the project activities that use percussives (not including tree removal/trimming or bridge/structure work) and/or increase noise levels above existing traffic/background levels consistent with a No Effect determination in this key?

Automatically answered

Yes, because the activities are within 300 feet of the existing road/rail surface, greater than 0.5 miles from a hibernacula, and conducted during the inactive season

29. Is the bridge removal, replacement, or maintenance activities portion of this project consistent with a No Effect determination in this key?

Automatically answered

Yes, because the bridge has been assessed using the criteria documented in the BA and no signs of bats were detected

Determination Key Description: FHWA, FRA, FTA Programmatic Consultation For Transportation Projects Affecting NLEB Or Indiana Bat

This key was last updated in IPaC on February 05, 2018. Keys are subject to periodic revision.

This decision key is intended for projects/activities funded or authorized by the Federal Highway Administration (FHWA), Federal Railroad Administration (FRA), and/or Federal Transit Administration (FTA), which require consultation with the U.S. Fish and Wildlife Service (Service) under Section 7 of the Endangered Species Act (ESA) for the endangered **Indiana bat** (*Myotis sodalis*) and the threatened **Northern long-eared bat** (NLEB) (*Myotis septentrionalis*).

This decision key should only be used to verify project applicability with the Service's [February 5, 2018, FHWA, FRA, FTA Programmatic Biological Opinion for Transportation Projects](#). The programmatic biological opinion covers limited transportation activities that may affect either bat species, and addresses situations that are both likely and not likely to adversely affect either bat species. This decision key will assist in identifying the effect of a specific project/activity and applicability of the programmatic consultation. The programmatic biological opinion is not intended to cover all types of transportation actions. Activities outside the scope of the programmatic biological opinion, or that may affect ESA-listed species other than the Indiana bat or NLEB, or any designated critical habitat, may require additional ESA Section 7 consultation.

Ryan Hennessey

From: Langevin, Toni N <TLangevin@indot.IN.gov>
Sent: Monday, February 12, 2018 9:55 AM
To: Ryan Hennessey
Subject: RE: USFWS Database Check: Des. No. 1401828, I-69 at S.R. 14, Allen County, IN

Hi Ryan,

A review of the USFWS database on February 12, 2018 for Des. No. 1401828 did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. Preparation of the Scoping Worksheet or use of the Information for Planning and Consultation (IPaC) database for the Range-Wide Programmatic Information Consultation for Indiana Bat and Northern Long-Eared Bat will be required. If applicable, preparation of the Project Submittal Form for Range-wide Programmatic Consultation for Indiana Bat and Northern Long-Eared Bat will be required.

Thank you,

Toni N. Langevin

Environmental Manager II

5333 Hatfield Road

Fort Wayne, IN 46808

Office: (260) 399-7341 x14341

Email: TLangevin@indot.in.gov



From: Ryan Hennessey [<mailto:ryanh@metricenv.com>]

Sent: Friday, February 09, 2018 11:32 AM

To: Langevin, Toni N <TLangevin@indot.IN.gov>; Novak, Karen <KNovak@indot.IN.gov>

Cc: Jessica Peterson <jessicap@metricenv.com>

Subject: USFWS Database Check: Des. No. 1401828, I-69 at S.R. 14, Allen County, IN

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Good Morning,

This email is to request information using the USFWS database regarding endangered bat habitat within the following project area.

- Des. No. 1401828, Intersection Modification, I-69 at S.R. 14, Allen County, IN

Attached is a Location and Aerial Map of the project.

Thank you,

Ryan Hennessey
Environmental Geologist

Phone: 317.608.2798 Email: ryanh@metricenv.com
6971 Hillsdale Court, Indianapolis, IN 46250

Northeastern Indiana Regional Coordinating Council

May 8, 2018

Jessica R. Peterson
Project Manager
Metric Environmental
6971 Hillsdale Court
Indianapolis, IN 46250



Re: Interchange Modification, I-69 at SR 14
DES #1401828
Location: Allen County, Indiana

Dear Ms. Peterson:

Members of our staff reviewed your letter and report, dated April 12, 2018, concerning the Early Coordination of the Interchange Modification project in Allen County. The NIRCC staff has the following comments relating to the adjacent trails with this project, see below.

- Currently, there is an existing trail west of the interchange that begins at Hadley Rd and extends west all the way to West Hamilton Rd and connects to a number of other trails that provide connectivity throughout the Aboite Township area. There is also an existing trail on the east side of the interchange that begins at Magnavox Way that extends toward the east where there is a planned trail project that will be under construction in the very near future to finish the connection all the way to downtown Fort Wayne, the Rivergreenway, and the Pufferbelly Trail (which is part of the Poka-Bache Connector trail from Pokagon State Park to Ouabache State Park). Any interchange modifications at SR 14 and I-69 should include trail infrastructure to complete this critical gap in the trail system. As for trail projects related to this specific project area though, unless this interchange modification includes adding a trail along with the project or there is another project to further modify this interchange in the near future that will include a trail with the project, then there is nothing planned to be constructed within the limits of this project in the next 2-4 years. This should be confirmed by Dawn Ritchie, City of Fort Wayne Greenways Manager, as well.
- In the red flag report there is a comment under the Infrastructure Summary that reads “Coordination with City of Fort Wayne Parks and Recreation will occur”. This should read that any planned trail that intersects the project area needs to be “coordinated with the City of Fort Wayne Public Works Department through Dawn Ritchie, Fort Wayne Greenways Manager”. Fort Wayne Parks and Recreation would not be the appropriate coordinating party with trails affected in this location.

Thank you for the opportunity to comment on this project. If you have any questions, please do not hesitate to contact our office.

Sincerely,

Stacey Gorsuch
Principal Transportation Planner

APPENDIX D: Section 106 of the National Historic Preservation Act

From: Miller, Shaun (INDOT)
To: [Jessica Peterson](#)
Cc: [Branigin, Susan](#); [Novak, Karen](#)
Subject: Re: Des. No. 1401828, Interchange Modification, SR 14 at I-69, Aboite Township, Allen County
Date: Sunday, April 1, 2018 5:28:18 PM
Attachments: [image001.png](#)
[image002.png](#)
[image003.png](#)
[image004.png](#)
[image005.png](#)

Hi Jessica,

Since all work will occur within the interchange, Category A-2 is most applicable.

Thank you,

Shaun Miller

From: Jessica Peterson <jessicap@metricenv.com>
Sent: Friday, March 30, 2018 12:57:58 PM
To: INDOT Cultural Resources
Cc: Branigin, Susan; Miller, Shaun (INDOT); Kumar, Anuradha
Subject: Des. No. 1401828, Interchange Modification, SR 14 at I-69, Aboite Township, Allen County

****** This is an EXTERNAL email. Exercise caution. DO NOT open attachments or click links from unknown senders or unexpected email. ******

Hello INDOT Cultural Resources,

Please see attached a packet containing exhibits and design plans for the above referenced project. Following is a synopsis of the project. Please provide your determination as to whether this project can be processed under Category A of the MPPA, as described below

Des. No.: 1401828

Project Location: SR 14 at I-69, Aboite Township Allen County, IN

Project Name: Interchange Modification

Project Description: The southwest ramp (loop) will be removed and traffic will be re-routed onto the northwest ramp. Two left-turn lanes will be constructed, in addition to the two existing right-turn lanes, where the northwest ramp merges onto SR 14. To accommodate heavier traffic, the eastbound segment of SR 14 will be expanded to include three lanes from the southwest I-69 on-ramp to the existing southwest I-69 off-ramp (loop, to be removed). No permanent or temporary right-of-way will be acquired.

MPPA Categories: A-2, A-4, and A-8

Thank you,

JESSICA R. PETERSON, MS
Project Manager

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Email: jessicap@metricenv.com
6971 Hillside Court, Indianapolis, IN 46250

www.metricenv.com



Category A consists of projects that, by their nature, have no effect on properties listed in or eligible for inclusion in the National Register of Historic Places (hereinafter referred to as the “National Register”) and do not require review by INDOT Cultural Resources Office. All of the work under this Category must occur in previously disturbed soils, which are defined as soils that have been completely altered or displaced by earthmoving or other modern manipulation.

1. Any work on bridges limited to substructure or superstructure elements without replacing, widening, or elevating the superstructure under the conditions listed below (***BOTH Conditions A and B must be met***). This category **does not** include bridge replacement projects (when both superstructure and substructure are removed):
 - A. The project takes place in previously disturbed soils; *AND*
 - B. With regard to the bridges, at least one of the conditions (i, ii or iii) listed below must be satisfied:
 - i. The bridge is not identified in the latest Historic Bridge Inventory as a National Register-listed or National Register-eligible (see <http://www.in.gov/indot/2531.htm>);
 - ii. The bridge was built after 1945, and is a common type as defined in Section V. of the *Program Comment Issued for Streamlining Section 106 Review for Actions Affecting Post-1945 Concrete and Steel Bridges* issued by the Advisory Council on Historic Preservation on November 2, 2012 for so long as that Program Comment remains in effect AND the considerations listed in Section IV of the Program Comment do not apply;
 - iii. The bridge is part of the Interstate system and was determined not eligible for the National Register under the Section 106 Exemption Regarding Effects to the Interstate Highway System adopted by the Advisory Council on Historic Preservation on March 10, 2005, for so long as that Exemption remains in effect.

2. All work within interchanges and within medians of divided highways in previously disturbed soils.

3. Replacement, repair, lining, or extension of culverts and other drainage structures in previously disturbed soils and do not exhibit stone or brick structures or parts therein.
4. Roadway work associated with surface replacement, reconstruction, rehabilitation, or resurfacing projects, including overlays, shoulder treatments, pavement repair, seal coating, pavement grinding, and pavement marking within previously disturbed soils where replacement, repair, or installation of curbs, curb ramps or sidewalks will not be required.
5. Repair, in-kind replacement or upgrade of existing lighting, signals, signage, and other traffic control devices in previously disturbed soils.
6. Repair, replacement, or upgrade of existing safety appurtenances such as guardrails, barriers, glare screens, and crash attenuators in previously disturbed soils.
7. Repair or in-kind replacement of fencing and hardscape landscaping elements and/or replacement of existing plant materials in previously disturbed soils and installation of new fencing and hardscape landscaping elements and plant materials limited to locations within interstate right-of way within previously disturbed soils.
8. Installation of new or modification of existing traffic control devices and systems, including signs, signals, markings, illumination, other warning devices and their supports, to improve safety at railway crossings in previously disturbed soils.

APPENDIX E: Red Flag and Hazardous Materials



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204-2216 (317) 232-5348 FAX: (317) 233-4929

Eric J. Holcomb, Governor
Joe McGuinness, Commissioner

Date: January 4, 2018

To: Hazardous Materials Unit
Environmental Services
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

From: Jessica Peterson, M.S.
Metric Environmental, LLC
6971 Hillside Court
Indianapolis, Indiana 46250
JessicaP@MetricEnv.com

Re: RED FLAG INVESTIGATION
Des. No. 1401828, Interchange Modification
I-69 at S.R. 14
Aboite Township, Allen County, Indiana

NARRATIVE

Indiana Department of Transportation proposes to utilize federal funds to modify an interchange at the intersection of Interstate (I-) 69 and State Road (S.R. 14) in Aboite Township, Allen County, Indiana, Designation Number (Des. No.) 1401828.

Specifically, the proposed project is in Sections 1 and 12, Township 30 North, Range 1 East and Section 6 and 7, Township 30 North, Range 12 East, as shown on the Fort Wayne West, Indiana 7.5 Minute United States Geological Survey (USGS) topographic quadrangle.

The need for this project is evidenced by the high traffic volumes on the southwest and southeast ramps weaving with eastbound through traffic on Illinois Road. American Association of State Highway and Transportation Officials (AASHTO) guidance recommends against adjacent loops when the sum of the traffic volumes on the two ramps exceeds 1,000 vehicles per hour (vph) because of the effect of weaving on mainline traffic. Current traffic counts show a combined AM peak-hour volume of 1,800 vph. The purpose of this project is to provide a safe roadway for motorists.

The proposed improvements include removing the southwest I-69 off-ramp loop, widening the northwest I-69 off-ramp, and modifying the median of S.R. 14. All work will occur within the existing right-of-way.

No additional right-of-way will be required for this project.

Maintenance of traffic will be conducted utilizing phased construction with shoulder closures and temporary lane closures.

SUMMARY

Infrastructure			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5 mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
Religious Facilities	N/A	Recreational Facilities	N/A
Airports	N/A	Pipelines	1
Cemeteries	N/A	Railroads	1
Hospitals	N/A	Trails	5
Schools	N/A	Managed Lands	N/A

Pipelines: One pipeline is located within the 0.5 mile search radius. The feature, a natural gas line owned by NIPSCO, intersects the south portion of the project area. Coordination with INDOT Utilities and Railroads will occur.

Railroads: One railroad is located within the 0.5 mile search radius. Norfolk Southern Railroad is located approximately 0.28 mile north of the project area. No impact is expected.

Trails: Five trails are located within the 0.5 mile search radius. One planned trail intersects the project area. Coordination with City of Fort Wayne Parks and Recreation will occur.

Water Resources			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5 mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
NWI - Points	N/A	NWI - Wetlands	19
Karst Springs	N/A	IDEM 303d Listed Lakes	N/A
Canal Structures – Historic	N/A	Lakes	4
NWI - Lines	3	Floodplain - DFIRM	44
IDEM 303d Listed Rivers and Streams (Impaired)	N/A	Cave Entrance Density	N/A
Rivers and Streams	2	Sinkhole Areas	N/A
Canal Routes - Historic	N/A	Sinking-Stream Basins	N/A
Urbanized Area Boundary (UAB)	1		

NWI – Lines: Three NWI Lines are located in the 0.5 mile search radius. The nearest feature, identified as R2UBH (Riverine, Lower Perennial, Unconsolidated Bottom, Permanently Flooded), is located approximately 0.14 mile southeast of the project area. No impact is expected.

Rivers and Streams: Two Rivers/Streams are located within the 0.5 mile search radius. Durnell Ditch intersects the project area. A Waters of the U.S. Report will be prepared and coordination with INDOT Environmental Services (ES) Ecology and Waterway Permitting Office (EWPO) will be conducted.

Urbanized Area Boundary (UAB): This project lies within the Fort Wayne UAB. Post construction Storm Water Quality Best Management Practices (BMPs) may need to be considered. An early coordination letter with topographic and aerial maps showing the project area should be sent to the Fort Wayne MS4 Coordinator at {brandi.wallace@cityoffortwayne.org}.

NWI – Wetlands: 19 NWI Wetlands are located within the 0.5 mile search radius. The nearest feature, identified as Palustrine, Unconsolidated Bottom, Excavated (PUBGx), is located approximately 0.05 mile southwest of the project area. No impact is expected.

Lakes: Four Lakes are located within the 0.5 mile search radius. The nearest feature, a perennial lake, is located approximately 0.03 mile southeast of the project area. No impact is expected.

Floodplain – DFIRM: 44 floodplain features are located within the 0.5 mile search radius. The nearest feature, the 100-year floodplain of Durnell Ditch, intersects the project area. Coordination with INDOT ES EWPO will be conducted.

Mining/Mineral Exploration			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5 mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
Petroleum Wells	N/A	Petroleum Fields	N/A
Mines – Surface	N/A	Mines – Underground	N/A

No Mining/Mineral Exploration features are located within the 0.5 mile search radius.

Hazardous Material Concerns			
Indicate the number of items of concern found within 0.5 mile and an explanation why each item within the 0.5 mile search radius will/will not impact the project. If there are no items, please indicate N/A:			
Brownfield Sites	N/A	Restricted Waste Sites	N/A
Corrective Action Sites (RCRA)	N/A	Septage Waste Sites	N/A
Confined Feeding Operations	N/A	Solid Waste Landfills	N/A
Construction Demolition Waste	N/A	State Cleanup Sites	N/A
RCRA Generators	2	Tire Waste Sites	N/A
Infectious/Medical Waste Sites	N/A	Waste Transfer Stations	N/A
Lagoon/Surface Impoundments	N/A	RCRA Waste Treatment, Storage, and Disposal Sites (TSDs)	N/A
Leaking Underground Storage Tanks (LUSTs)	3	Underground Storage Tanks (USTs)	2
Manufactured Gas Plant Sites	N/A	Voluntary Remediation Program	N/A
NPDES Facilities	N/A	Superfund	N/A
NPDES Pipe Locations	N/A	Institutional Control Sites	1
Open Dump Sites	N/A		

RCRA Generators: Two RCRA Generators are located within the 0.5 mile search radius. The features, Hires Auto Part and Lincoln National Corporation, are located approximately 0.4 mile east and southeast of the project area, respectively. Both facilities are IDEM Small Quantity Generators. No impact is expected.

Leaking Underground Storage Tanks (LUSTs): Three LUSTs are located within the 0.5 mile search radius. The nearest site, Herdrich Petroleum-Cummings III (Agency Interest (AI) Identification (ID): 2532, 6310 Illinois Road), is located approximately 0.13 mile east of the project area. According to the Indiana Department of Environmental Management (IDEM) Virtual File Cabinet (VFC), the site was a former gas station which was found to be contaminated with hydrocarbon contaminants of concern (COCs). After site closure, soil and groundwater analysis indicated concentrations of COCs had fallen below closure levels. A No Further Action letter was issued on February 21, 2002. No impact is expected.

Underground Storage Tanks (USTs): Two USTs are located within the 0.5 mile search radius. The nearest site, Finish Line Car Wash (AI ID 4585, 6302 Illinois Road), is located approximately 0.13 mile east of the project area. According to the IDEM VFC, the facility is now B & B Reality, LLC. Although violations have occurred, no releases have been recorded for the site. During the most recent inspection, conducted on September 8, 2016, the facility was in compliance with all UST guidelines. No impact is expected.

Institutional Control Sites: One Institutional Control site is located within the 0.5 mile search radius. The feature, Speedway No. 5161, is also associated with a LUST. Please refer to the LUST section of this document for more information.

Ecological Information

The Allen County listing of the Indiana Natural Heritage Data Center information on endangered, threatened, and rare (ETR) species and high quality natural communities is attached with ETR species highlighted. Early coordination will be undertaken with the IDNR Division of Fish and Wildlife to determine if any plant or animal species listed as state or federally ETR have been reported to occur within the project area.

A review of the USFWS database did not indicate the presence of endangered bat species in or within 0.5 mile of the project area. Preparation of the Scoping Sheet for the Indiana Bat and Northern Long-Eared Bat Range-Wide Programmatic Information Consultation will be necessary. If applicable, preparation of the Project Submittal Form for Range-wide Programmatic Consultation for Indiana Bat and Northern Long-Eared Bat will be required.

An inquiry using the USFWS Information for Planning and Consultation (IPaC) website did not indicate the presence of the federally endangered species, the rusty patched bumblebee, in or within 0.5 mile of the project area.

Cultural Resources

Metric reviewed the Indiana State Historic Architectural and Archaeological Research Database (SHAARD) and identified 43 County Survey Sites. The nearest site is located approximately 0.05 mile northwest of the project area. No impact is expected.

Based on the scope of work, this project appears to fall under Category A-3 of the *Programmatic Agreement among the Federal Highway Administration, INDOT, the Advisory Council on Historic Preservation, and the Indiana State Historic Preservation Officer Regarding the Implementation of the Federal Aid Highway Program in the State of Indiana* (MPPA). Coordination with INDOT Cultural Resources Office (CRO) will be undertaken for verification.

RECOMMENDATIONS

INFRASTRUCTURE:

Pipelines: Coordination with INDOT Utilities and Railroads will occur.

Trails: Coordination with City of Fort Wayne Parks and Recreation will occur.

WATER RESOURCES: Durnell Ditch and its 100-year floodplain intersect the project area. A Waters of the U.S. Report will be prepared and coordination with INDOT ES EWPO will be conducted.

MINING/MINERAL EXPLORATION: N/A

HAZMAT CONCERNS: N/A

ECOLOGICAL INFORMATION: Early coordination will be undertaken with the IDNR Division of Fish and Wildlife to determine if any plant or animal species listed as state or federally ETR have been reported to occur within the project area. Preparation of the Scoping Sheet for the Indiana Bat and Northern Long-Eared Bat Range-Wide Programmatic

Information Consultation will be necessary. If applicable, preparation of the Project Submittal Form for Range-wide Programmatic Consultation for Indiana Bat and Northern Long-Eared Bat will be required.

CULTURAL RESOURCES: Based on the scope of work, this project appears to fall under Category A-3 of the MPPA. Coordination with INDOT CRO will be undertaken for verification.

INDOT Environmental Services concurrence: Nicole Fohey-Breting (Signature)

Digitally signed by Nicole Fohey-Breting
DN: cn=Nicole Fohey-Breting, o=INDOT,
ou=Environmental Services, HazMat,
email=NFoheyBreting@indot.in.gov,
c=US
Date: 2018.01.05 08:39:24 -05'00'

Prepared by:
Jessica Peterson, M.S.
Environmental Scientist
Metric Environmental, LLC

Graphics:

GENERAL SITE MAP SHOWING PROJECT AREA: YES **Duplicate topographic map removed. See Appendix B-2.**

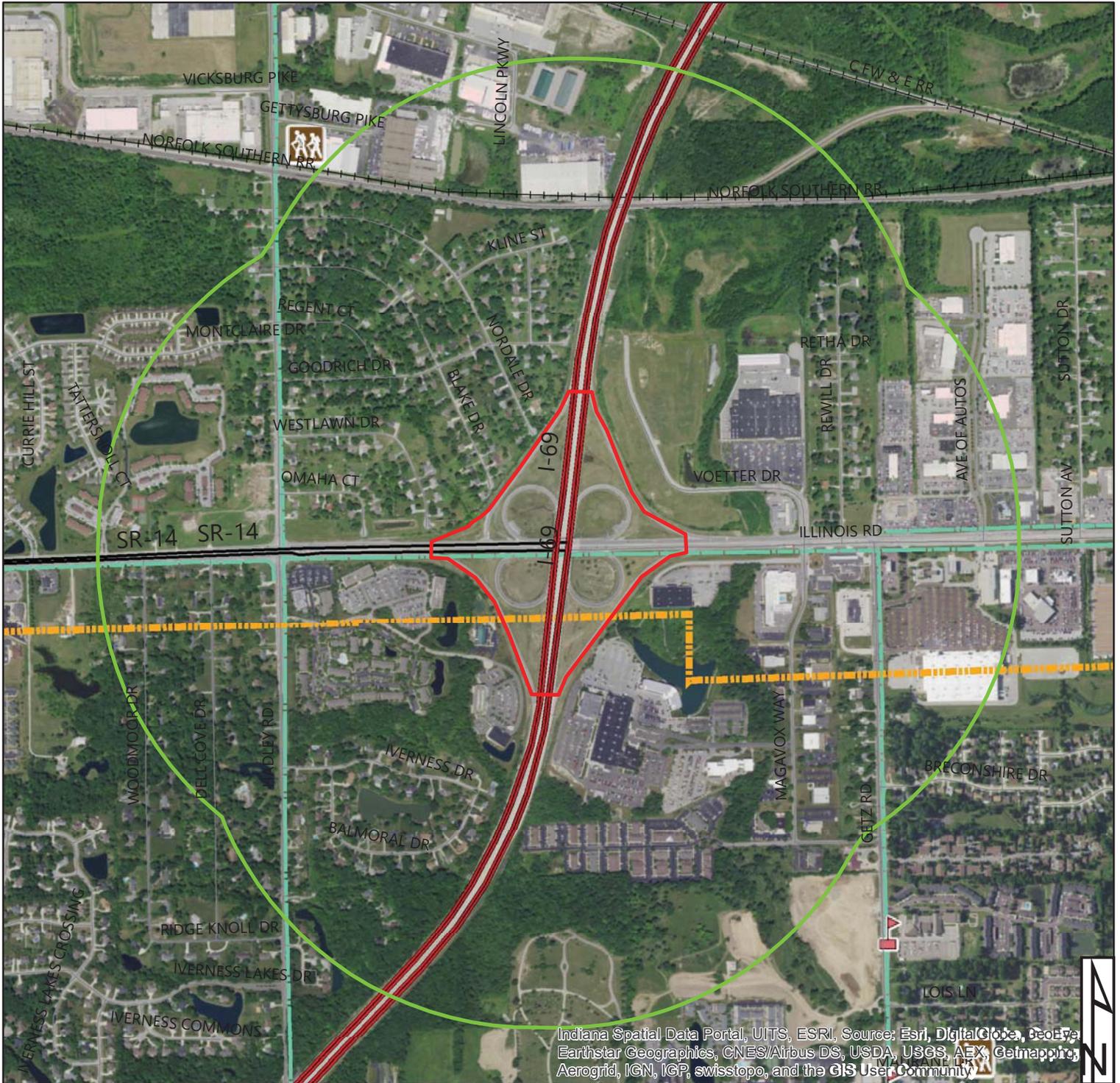
INFRASTRUCTURE: YES

WATER RESOURCES: YES

MINING/MINERAL EXPLORATION: NO

HAZMAT CONCERNS: YES

Red Flag Investigation - Infrastructure Map
 I-69 at S.R. 14
 Des. No. 1401868, Interchange Modification
 Aboite Township, Allen County, Indiana



0.25 0.125 0 0.25 Miles

Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

	Religious Facility		Recreation Facility		Project Area
	Indiana Map		Pipeline		Half Mile Radius
	Religious Facility		Railroad		Interstate
	Airport		Trails		State Route
	Cemeteries		Managed Lands		US Route
	Hospital		County Boundary		Local Road
	School				